

Epping Forest Local Highway Panel Meeting Agenda

Date: Thursday 16th March 2017

Time 18:00 pm

Venue: Council Chamber, Civic Offices,
High Street, Epping

Chairman: Cllr J Knapman (ECC)

Panel Members Cllr R Gadsby (ECC), Cllr A Jackson (ECC), Cllr V Metcalfe (ECC), Cllr M McEwen (ECC), Cllr C Pond (ECC), Cllr J Whitehouse (ECC), Cllr R Bassett, Cllr G Chambers, Cllr K Chana, Cllr C Roberts, Cllr P Keska, Cllr N Avey and Cllr E Webster.

Other Attendees Matt Valentine - Highways Liaison Officer, Sonia Church - Highways Liaison Manager. Anthony Belgrave – Secretariat.

Time		Item	Lead:	Papers:
	1.	Welcome and introduction	Chairman	Verbal
	2.	Apologies for Absence	Chairman/ MV	Verbal
	3.	Declarations of Interest	All	Verbal
	4.	Minutes of meeting held on 13 th December 2016 to be agreed as correct record.	Chairman	Attached
	5.	Indicative Programme list for Schemes Approved and Delivered from 2012/13 to 2016/17	MV	Attached
	6.	To consider and determine those schemes which should be recommended or removed from the potentials scheme list for the 2017/18 financial year. <ul style="list-style-type: none"> • Passenger Transport • PROW • Traffic Management • Walking 	Chairman/ MV	Attached
	7.	S106/Rangers Update.	MV	Attached for information
	8.	Appendix	MV	Attached for information
	9.	AOB Fingerposts. Dates for Next Meetings; Tuesday 20 th June 2017 Tuesday 12 th September 2017 Tuesday 16 th January 2018	All	Verbal

ECC/EFDC LOCAL HIGHWAY PANEL MINUTES

Committee: ECC/EFDC Local Highways Panel **Date:** Tuesday 13th December 2016

Place: Committee Room 2, Civic Offices, High Street, Epping **Time:** 18:00 – 20:00

Members Present: **County Councillors:**

Councillors J Knapman, A Jackson, Mrs V Metcalfe, C Pond and J M Whitehouse.

District Councillors:

Councillors R Bassett, E Webster and K Chana.

Other Councillors: Councillors G Waller and C Pond.

Apologies: County Councillor M McEwen and R Gadsby. District Councillor, G Chambers, C Roberts, P Keska and N Avey.

Officers Present: M Valentine (Highways Liaison Officer (HLO) - Ringway Jacobs | Essex County Council) and S Church (Highways Liaison Manager – Ringway Jacobs | Essex County Council), Anthony-Louis Belgrave (Chigwell Parish Council)

1. WELCOME AND INTRODUCTION

The Chairman, Councillor J Knapman welcomed those Members present and those attending in an observational role.

2. DECLARATIONS OF INTEREST SCHEME RECOMMENDATIONS 2016/17

There were no declarations of interest pursuant to the Council's Code of Member Conduct.

3. MINUTES

RESOLVED:

That the minutes of the meeting held on Tuesday 6th September 2016 be taken as read and signed by the Chairman as a correct record.

4. OAKWOOD HILL (LEP152003) - UPDATE

The Chairman, and Local Highway Panel (LHP) were advised by the Highways Liaison Officer of the present cost of the aforementioned scheme. They were further advised as to the reasoning for the underestimation of the cost to complete this scheme. The allocated budget previously stood at £14,000 whilst the present cost is now estimated to be £18,504.69. A thorough and detailed discussion took place, during which the HLO explained the tender process and quantitative analysis procedure for such schemes. The Chairman and members of the LHP also expressed severe questions for the apparent absence of proper monetary accountability in respect of such schemes and the gross underestimation of the Oakwood Hill scheme in particular. After further discussion it was agreed the scheme should proceed as described and that the Chairman would correspond with the Cabinet Member for Highways and Transport to convey the concerns expressed by the LHP.

5. SCHEME RECOMMENDATIONS 2017/18 FINANCIAL YEAR

The Chairman, HLO and members of the LHP considered the 2017/18 Scheme list to determine which potential schemes should be commissioned within the 2017/18 financial budget and those to be removed.

(1) Cycling

To be deferred to 2017/18.

(2) Passenger Transport

Item 2 – Bushfields, Loughton; Install metal bus shelter. To be progressed.

Item 3 – Colson Road, Loughton; Install a bus cage road marking. To be progressed.

Item 4 – Bushfields, Loughton; Install a bus cage road marking. To be progressed.

Item 5 – Newmans Lane, Loughton (Hatfields) – PT; Install bus cage road markings. To be progressed.

Item 6 – 42001003 Stops at Baldwins Hill, Loughton – PT; Increase hardstand area, provide barriers and general bus stop improvements. After discussion it was agreed that this project would remain on the scheme list and be re-considered at the meeting in March 2017.

(3) Public Rights of Way

Item 6 – Footpath 18 – Staples Hill, Loughton – PROW Improvements; Resurface the footpath. To be progressed.

Item 7 – Public Footpath 109, Loughton. Known locally as the ‘Oatley Path’ – PROW Improvements. Surfacing works are required along the path to the rear of Harwater Drive. To be progressed.

Item 8 – Bridleway 16, High Ongar. Lay a layer of crushed concrete to provide long lasting firm base and to raise existing surface slightly above surrounding ground level. Top with plantings and compact to provide suitable surface for all users. To be progressed.

(4) Safer Roads

Item 1 – St. Leonards Road/Nazeing Road/North Street/Middle Street, Lower Nazeing; highway widening project. After discussion it was agreed that this project would be removed from the potential schemes list.

(5) School Crossing Patrol

No Change.

(6) Traffic Management

Item 1 – B172 Abridge over River Roding –TMI. Introduce warning signage on the approach to the bridge (highlighting that the road narrows and that oncoming vehicles would be in the middle of the road.) Remove the centre line marking where the carriageway reduces to less than 5.5 metres in width – In line with the northern end of the bridge and around 10 metres to the south of the bridge. Introduce ‘solar light’ studs adjacent to the carriageway edge throughout the area where the centre line marking is removed. Replace the existing double yellow lining along the B172 Abridge Road with 50mm wide primrose ‘conservation’ lining to accommodate the proposed road studs. To be progressed.

Item 2 – Salt Bins – Loughton. Salt Bin requests. After discussion it was agreed that this project would be removed from the potential schemes list.

Item 3 – B1393 High Road/Theydon Road, Epping – Studies for junction widening. Ecology Impact assessment and Land acquisition investigation to determine if junction widening can be carried out. After discussion it was agreed that this project would be removed from the potential schemes list.

Item 4 – Station Road, Chigwell – Design for road widening. Legal Fees for land acquisition, topographical survey and detailed design for widening Station Road. After discussion it was agreed that this project would be removed from the potential schemes list.

Item 5 – Pyres Lane, Chester Road, Hillyfields – Implementation of a speed table; Removing the existing material and replacing with new material to current road specification. Remove old warning signs related to the junction and install new 'staggered junction ahead, warning signs on yellow backing board plus new posts along Chester Road and Hillyfields. Install two new grey backed give way 'X' yards ahead warning sign on Pyrles Lane (north and south) with new posts. Take down some of the existing guard railing which is sited within the visibility splays for Pyrles Lane give way position and replace with PG2 type guard railing. After discussion it was agreed that this project would remain on the potential schemes list.

Item 6 – All roads leading into and through the forest – Signage review, Epping Forest; Signage investigation to direct any 'lost' HGV drivers to the more suitable roads. After discussion it was agreed that this project would remain on the potential schemes list.

Item 8 – Lower Road, Loughton – A feasibility study to determine if Lower Road can be a 'one way' road. After discussion it was agreed that this project should progress.

Item 9 – Loughton Way j/w River Road. Buckhurst Hill- dropped kerbs. Install dropped kerbs with tactile paving and pedestrian refuge. To be progressed.

Item 10 – Laundry Lane; no through road or install lockable gate; after discussion it was agreed that this project would remain on the potential schemes list and would be further discussed at a subsequent Highways Surgery.

Item 11 – Upshire Road, Waltham Abbey – TMI. For safety measures and packing bays by Upshire Primary School. After discussion it was agreed that this project would be removed from the potential schemes list.

Item 12 – B181 Lindsay Street, Epping – VAS Sign; Install a VAS Sign by Shaftsbury Farm going west. After discussion it was agreed that the respective Validation report concerning this project would be made available to the LHP by the HLO.

Item 13 – Tidy's Lane, Epping TMI; Feasibility study to look at the options to prevent the verge from being torn up along Tidy's Lane. After discussion it was agreed that the pursuance of a solution to the issues described would be a more efficient endeavour than a feasibility study.

Item 14 – Hoe Lane, Nazeing – TMI; A detailed design to be created with a view to look into installing kerbs and appropriate drainage from Tayness and opposite Parkers Farm. To include a topographical survey. To be progressed.

Item 15 – Powdermill Lane j/w Leaview, Waltham Abbey – Overrun area; detailed design to determine whether an overrun could be constructed. To be progressed.

Item 16 - Pump Hill, Loughton – Signage; Install ‘give way’ triangle to highlight the give way markings. To be progressed.

Item 17 – Kendal Avenue, Epping – TMI; Traffic Calming measures. After discussion it was agreed that this project would be removed from the potential schemes list.

Item 18 – The Street j/w Crown Close, Sheering – TMI; To add deflection at the mini-roundabout with both physical measures and ghost white lined islands directing the traffic to drive around the dome instead of straight over it. A new dome should be installed in the centre of the mini-roundabout at only 2m diameter to permit the safe passage of public service vehicles (bus). Relocate the warning sign on the eastern stretch of The Street closer to the junction. This will give drivers enough warning of the junction ahead instead of notifying them more than 100m in advance of the roundabout as it is existing. After discussion it was agreed that this project be progressed but the installation of the dome would be abandoned.

Item 19 – Willingale Road, Loughton – TMI; A feasibility study to be carried out to determine whether the build outs can be improved, or replaced with other traffic calming methods. To be progressed.

Item 20 – Baden Drive, Waltham Abbey – TMI; Install junction protection marking for 15m. To be progressed.

Item 22 – A113 London road, Stanford Rivers – Speed limit; Install a 40mph buffer zone, move the existing 30mph limits to a more appropriate location, carry out the Traffic Regulation Order and formal consultation. To be progressed.

Item 23 – Station Approach, Loughton – Bollards; to install 37 bollards at 1.5m apart to stop cars parking on Station Approach. To be progressed.

(7) Quiet Lanes

No Change

(8) Walking

No Change

(9) Potential Revenue

No Change

RESOLVED:

That the 2017/18 Scheme Recommendation list be altered in compliance with the suggestions from the members of the LHP, and possible future schemes be proposed by members at subsequent meetings.

6. INDICATIVE PROGRAMME LIST

The HLO advised members of the Highway Panel on the schemes approved and delivered from 2012/13 to 2016/17.

That this information be **NOTED**.

7. S106/RANGERS UPDATE

The HLO advised members of the Highway Panel on the completed schemes, and those presently being undertaken by the Highway Rangers.

That this information be **NOTED**, and the HLO facilitate the attendance of a senior representative of the Highway Rangers at a subsequent LHP meeting if required.

8. ANY OTHER BUSINESS

(1) Accountability

After discussion the Chairman expressed concerns of the lack of transparency in the budgetary processes in respect of approved schemes. This concern was reiterated by members, and after further discussion it was agreed that the Chairman would correspond with the respective Portfolio Holder to investigate the feasibility of more transparency with respect to how the allocated budget is calculated for suggested, as well as approved schemes.

(2) LHP Terms of Reference

The HLO advised members of the new LHP Terms of Reference that is available on the ECC website.

9. FUTURE MEETING ARRANGEMENTS

Members agreed that the next meeting of the LHP would be held at the Civic Offices on Thursday 16th March 2017 at 6.00pm.

RESOLVED:

(1) That the next meeting of the Local Highways Panel would be held at the Civic Offices on Thursday 16th March 2017 at 6.00pm.

CHAIRMAN

Epping District Approved Scheme List

Key:	Completed schemes
	Live Schemes
	2017/18 Financial year

Ref	COST CODE	SCHEME NAME	CURRENT STAGE	ANTICIPATED COMPLETION DATE	WORK DESCRIPTION	ALLOCATED BUDGET	COMMENTS
2014/15 Approved Schemes							
1	LEPP142011	War Memorial Lindsey Street, Epping	N/A	tbc	Change link road fronting War Memorial to footway and associated junction improvements at Lindsey Street/High Street	£35,000	The scheme request will be presented to the Epping Forest & Commons Committee in May 2017, where a decision will be made on a way forward.
2015/16 Approved Schemes							
2	LEPP151001	A1168 Chigwell Lane j/w M11 northbound off-slip (M11 jct 5)		2017/18 Financial year	Install 'Give Way' sign; Install advanced lane merging into single carriageway sign and construct a build out along Chigwell Lane.	£21,000	There is a major project within the same vicinity and therefore the scheme has had to move into Quarter 2 of the 2017/18 financial year.
3	LEPP151004	B181 Epping Road j/w Common Road, Broadley Common	Completed	20/12/2016	Replace the existing partial Bennett junction with a standard priority junction consisting of a small central traffic island; Re-align the kerblines to the east and west of Common Road close to the junction with B181 Epping Road; Relocate/provide a new yellow backed 'give way' sign to the nearside of the carriageway on the Common Road approach; Review all existing local directional signs at the junction and ensure that appropriate new signs are provided	£12,000	
4	LEPP151009	Sedge Green j/w Dobbs Weir Road, Nazeing		2017/18 Financial year	Install bollards on corner of junction.	£2,000	Can cancel if panel wish, as doesn't meet new criteria for Casualty Reduction schemes
5	LEPP151011	Coopersale Street, bend 200m south east of Houbbons Hill	N/A	tbc	Renew the existing northbound 'bend ahead' with 'reduce speed now' sub-plate signs; Install new chevron signs for both directions at the bend; Renew remaining reflector posts and install additional posts around the sweeping bend.	£20,000	Land acquisition needed for Coopersale Street works - The land owners have involved their solicitors and are requesting financial compensation.
6	LEPP151015	A121 Woodridden Hill approach to Wakes Arms Roundabout	Completed	10/03/2017	Install new roundabout ahead warning sign; Install new SLOW markings; Replace the existing chevron sign on the roundabout island; Check distances between 40mph repeaters and if additional signs can be installed.	£2,300	
7	LEPP152001	Church Road, Buckhurst Hill	N/A	tbc	Letter drop for informal consultation with residents for possible one way system.	£1,500	Does the panel still support this request.
8	LEPP152003	Oakwood Hill, between River Way and Lower Alderton Hall Lane	In Progress	Quarter 2 2017/18	Improve pedestrian refuge. Upgrade signing, lining and lighting units	£14,000	Due to the permit team requesting the delivery during the Summer holidays
9	LEPP152027	A121 Goldings Hill, Loughton, junction with A1168 Rectory Lane	In Progress	20/03/2017	Install 2 x zebra crossing warning signs with distance sub plates. Install lane lining with directional arrows.	£10,000	
10	LEPP152046	Debden Lane, Loughton	In Progress	Quarter 1 2017/18	Install 2 VAS Westbound and EastBound; Install extra 30mph repeater signs; Install extra chevron signs with yellow borders; Replace existing chevron signage with chevrons with yellow borders; Install 30mph roundels; Install 'SLOW' markings adjacent road.	£28,000	
11	LEPP152050	B181/Upland Road, Epping Upland	Completed	07/03/2017	FEASIBILITY STUDY - To investigate a mini-roundabout at the T junction. Improved signage to delineate the B181/Upland Road better and to guide vehicles along the B181.	£3,000	
12	LEPP153001	Hemnal Street, Epping	In Progress	tbc	Footway and dropped kerbing.	£7,500	The Epping Forest & Commons Committee never granted permission and therefore discussions are needed with the CoL on a way forward.

Ref	COST CODE	SCHEME NAME	CURRENT STAGE	ANTICIPATED COMPLETION DATE	WORK DESCRIPTION	ALLOCATED BUDGET	COMMENTS
2016/17 Approved Schemes							
13	LEPP161001	Alderton Hill j/w Alderton Hall Lane, Loughton	Completed	26/01/2017	<ul style="list-style-type: none"> 1) Investigate the feasibility of installing a ghost island in the bellmouth of Alderton Hall Lane. This is to help highlight the junction ahead to approaching motorists. 2) Renew the existing give way sign on Alderton Hall Lane on yellow backing board. 3) Install a new yellow backed advanced give way sign on the approach to the junction on Alderton Hall Lane. 4) Investigate the possibility of installing direction flag signs opposite the junction if there is sufficient room. This is aimed at further highlighting the junction to approaching motorists, particularly at night. 6) Street lighting to check the existing lighting levels at the junction. 	£20,000	
14	LEPP161002	B181 Epping Upland by Chambers Manor	Completed	17/02/2017	<ul style="list-style-type: none"> 1) Install yellow backed bend warning sign on B181 southbound approach with a 'Max Speed' sub-plate. 2) Replace both of the existing chevron signs for southbound traffic on the B181 with yellow backing boards (only one is currently yellow backed and the border is quite narrow). 3) Replace the existing chevron sign for westbound traffic on the B181 with one with a yellow backing board and cut back the adjacent vegetation surrounding it. 4) If there is sufficient verge width, install an additional yellow backed chevron for southbound traffic on the B181 to the outside of the unnamed road junction. 5) Add road studs around the centre line of the bend on the B181. 	£15,000	
15	LEPP161003	Daws Hill j/w A112 Sewardstone Rd, Sewardstone	In Progress	2017/18 programme	<ul style="list-style-type: none"> 1) To investigate the feasibility/cost of removing the Bennett Island. There are various statutory undertaker's equipment and street furniture on the island which could potentially be costly to move. 2) If Bennett Island is to remain then the signing at the junction needs replacement/upgrading to reduce the clutter and ensure it is visible (yellow back the give way sign and replace/combine the flag signs). 3) Investigate the cost to move/replace the lamp column by the access into Woodlands (north of the junction) to the back of the verge so it doesn't affect the sightline from Daws Hill. 4) Replace the missing section of the northbound bend with junction warning sign and move the sign assembly so that it is not masked by the VAS sign. 	£80,000	
16	LEPP161004	Reeves Lane jw Tylers Road, Roydon Hamlet	Completed	17/02/2017	<ul style="list-style-type: none"> 1) Replace the chevron signing at the bend which has been hit with yellow backed signs. 2) Renew the existing bend with junction ahead warning sign for eastbound traffic on yellow backing board positioned further from bend. Also add 'Max Speed 30' sub-plate. 3) Install new reflector posts around the outside of the bend. 4) Arrange for a scrim survey to be completed to check the skid resistance of the carriageway surface around the bend. 5) Drainage system to be checked to ensure it is working correctly. 6) Missing road studs around bend to be replaced. 7) Check that repeater signing for 40mph speed limit is sufficient and enforceable as there appears to be a lack of repeater signs. 	£25,000	
17	LEPP162001	North St, Pecks Hill, Sedge Green, Dobbs Weir Road.	2017/18 Financial year		A review is to be carried out looking into the signage of the weight limit.	£5,000	
18	LEPP162002	Middle Street, Nazeing - South of golf course entrance	Completed	27/01/2017	Install chevron signs on the S bend to aid drivers.	£6,000	
19	LEPP162003	Church Lane and Harlow Road, Sheering	2017/18 Financial year		3 x advance signage to be installed on both approaches to Church Lane reading 'Unsuitable for HGVs'.	£4,000	
20	LEPP162004	Honey Lane (east of Wood Green Road), Waltham Abbey	Completed	06/03/2017	Option study to aid pedestrians to cross the road.	£5,000	
21	LEPP162006	Victoria Road, Buckhurst Hill	2017/18 Financial year		Install a dropped kerb along Victoria Road by Pegasus Court.	£3,500	
22	LEPP162008	High Road j/w Bassett Gardens, North Weald Bassett	2017/18 Financial year		Install 15 guardrails to prevent parking on the zig zag lines adjacent to the pedestrian crossing.	£12,000	
23	LEPP162011	Forest Road j/w Smarts Lane, Loughton	2017/18 Financial year		To install additional bollards along the footway for a distance of 54m, from the first property in Forest Road to the junction of Smarts Lane. Install NWAAT restriction just passed the priority working and install junction protection markings at Smarts Lane.	£15,000	
24	LEPP162012	A121 High Road, Loughton	2017/18 Financial year		Install signage to guide vehicles to the car park, Library and National Jazz Archive.	£4,000	
25	LEPP162013	Clare Mews, Ongar	In Progress	31/03/2017	Install 'Keep Clear' Markings and name plate signage.	£2,500	
26	LEPP162016	Motts Lane, Waltham Abbey	2017/18 Financial year		Signage review to be carried out to work out what can be done to increase the visibility of the existing signage to lorry drivers.	£2,000	
27	LEPP162017	Kiln Road, Bassett Gardens, Wheelers Farm Gardens and Harrison Drive, North Weald Bassett	In Progress	18/04/2017	Install 4 dropped kerbs with tactile paving.	£17,000	

Ref	COST CODE	SCHEME NAME	CURRENT STAGE	ANTICIPATED COMPLETION DATE	WORK DESCRIPTION	ALLOCATED BUDGET	COMMENTS
28	LEPP162020	Brook Road, Epping	2017/18 Financial year		Install a blue advanced height warning sign to warn vehicles of the bridge	£2,000	
29	LEPP162021	Ivy Chimney Road, Epping	Completed	10/03/2017	FEASIBILITY STUDY - to determine options to improve the parking issue along the footway.	£3,000	
30	LEPP162022	Staples Road / Shaftesbury, Loughton	2017/18 Financial year		Replace the existing wooden gate with a metal gate	£7,500	
31	LEPP162024	A113 Romford Road	2017/18 Financial year		The existing kerb inlet gullies are replaced. An additional gully and ditch re-grading is provided at the carriageways low spot to eliminate further ponding and eventual flooding.	£12,000	
32	LEPP162025	Watery Lane, Little Laver	2017/18 Financial year		Implementation of a Quiet Lane.	£6,000	
33	LEPP166001	Near 79 High road, Buckhurst Hill	2017/18 Financial year		Upgrade the flashing school warning sign.	£4,000	
2017/18 Approved Schemes (Provisional)							
34	LEPP165004	Bushfields, Loughton - 1500IM399B (parkmead) - PT	LHP decision to be ratified at March 2017 LHP		Install metal bus shelter	£6,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
35	LEPP165007	Colson road, Loughton - 150042014010 (chequers Road) - PT	LHP decision to be ratified at March 2017 LHP		Install bus cage road markings	£2,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
36	LEPP165008	Bushfields, Loughton - 1500IM399B (parkmead) - PT	LHP decision to be ratified at March 2017 LHP		Install bus cage road markings	£2,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
37	LEPP165009	Newmans Lane Loughton - 150042013003 (Hatfields) - PT	LHP decision to be ratified at March 2017 LHP		Install bus cage road markings	£2,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
38	LEPP168006	Footpath 18 - Staples Hill, Loughton - PROW Improvements	LHP decision to be ratified at March 2017 LHP		Resurface the footpath	£10,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
39	LEPP168007	Public Footpath 109 Loughton. Known locally as the "Oatley Path"- PROW Improvements	LHP decision to be ratified at March 2017 LHP		Surfacing works are required along the path to the rear of Harwater Drive	£15,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
40	LEPP168008	Bridleway 16 High Ongar	LHP decision to be ratified at March 2017 LHP		Lay a layer of crushed concrete to provide long lasting firm base and to raised existing surface slightly above surrounding ground level. Top with planings and compact to provide suitable surface for all users	£12,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
41	LEPP162060	B172 Abridge over river Roding - TMI	LHP decision to be ratified at March 2017 LHP		Introduce warning signage on the approach to the bridge (highlighting that the road narrows and that oncoming vehicles would be in the middle of the road.) Remove the centre line marking where the carriageway reduces to less than 5.5 metres in width - In line with the northern end of the bridge and around 10 metres to the south of the bridge. Introduce 'solar lite' studs adjacent to the carriageway edge throughout the area where the centre line marking is removed. Replace the existing double yellow lining along the B172 Abridge Road with 50mm wide primrose 'conservation' lining to accommodate the proposed road studs.	£8,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
42	LEPP162014	Lower road, Loughton - Feasibility for one way road	LHP decision to be ratified at March 2017 LHP		A feasibility study to Investigate if Lower Road can be a 'one way' road.	£3,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
43	LEPP162015	Loughton Way j/w River road, Buckhurst Hill - Dropped kerbs	LHP decision to be ratified at March 2017 LHP		Install dropped kerbs with tactile paving and a pedestrian refuge.	£4,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
44	LEPP162061	Hoe Lane, Nazeing - TMI	LHP decision to be ratified at March 2017 LHP		A detailed design be created with a view to look into installing kerbs and appropriate drainage from Tayness and opposite Parkers Farm. To include a topographical survey.	£5,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting

Ref	COST CODE	SCHEME NAME	CURRENT STAGE	ANTICIPATED COMPLETION DATE	WORK DESCRIPTION	ALLOCATED BUDGET	COMMENTS
45	LEPP162032	Powdermill Lane j/w Leaview, Waltham Abbey - Overrun area	LHP decision to be ratified at March 2017 LHP		Detailed design to determine whether an overrun area could be constructed.	£3,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
46	LEPP162063	The Street j/w Crown Close, Sheering - TMI	LHP decision to be ratified at March 2017 LHP		To add deflection at the mini roundabout, with both physical measures and ghost white lined islands directing the traffic to drive around the dome instead of straight over it. A new dome should be installed in the centre of the mini roundabout at only 2m diameter to permit the safe passage of public service vehicles (bus). Relocate the warning sign on the eastern stretch of The Street closer to the junction. This will give drivers enough warning of the junction ahead instead of notifying them more than 100m in advance of the roundabout as it is existing.	£21,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
47	LEPP162028	Willingale Road, Loughton - TMI	LHP decision to be ratified at March 2017 LHP		A feasibility study be carried out to determine whether the build outs can be improved or replaced, with other traffic calming methods.	£4,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
48	LEPP162032	Baden Drive, Waltham Abbey - TMI	LHP decision to be ratified at March 2017 LHP		Install junction protection marking for 15m	£3,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
49	LEPP162034	A113 London road, Stanford Rivers - Speed limit	LHP decision to be ratified at March 2017 LHP		Install a 40mph buffer zone, move the existing 30mph limits to a more appropriate location, carry out the Traffic Regulation Order and formal consultation.	£8,000	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting
50	LEPP162035	Station Approach, Loughton - Bollards	LHP decision to be ratified at March 2017 LHP		To install 37 Bollards at 1.5m apart to stop cars parking on Station Approach.	£8,500	2017/18 recommendations from Potential Schemes List at December 2016 Panel meeting

**EPPING FOREST DISTRICT COUNCIL WITH ESSEX COUNTY COUNCIL
LOCAL HIGHWAY PANEL – 16th MARCH 2017
POTENTIAL CAPITAL SCHEMES**

As part of the Essex County Council 2017/18 budget it is believed that the Capital budget for the Epping Forest Local Highways Panel (LHP) will remain the same as that for 2016/17. This would mean for Epping Forest LHP the available 2017/18 capital budget will be £349,774. It is recommended for 2017/18 that the Panel make further scheme funding recommendations to create a £450,000 rolling programme of highway improvement works.

This Potential Scheme List identifies all of the scheme requests which have been received for the consideration of the Epping Forest LHP. The Panel are asked to review the schemes on the attached Potential Scheme List, prioritising those schemes they would like to make 2017/18 funding recommendations against and remove any schemes the Panel would not wish to consider for future funding.

2017/18 Budget Summary	Amount
Recommended Rolling Programme total	£450,000
Programmed Works (on Approved Works Programme into 2017/18)	£191,500
2017/18 Casualty Reduction schemes	£35,000
Total amount available for the LHP to allocate in 2017/18	£223,500
2017/18 Schemes recommendations from December Panel meeting	£119,500
Amount remaining for the LHP to allocate in 2017/18	£104,000

Currently the total value of potential schemes within the Epping Forest District stands at £309,000.

The RAG column acknowledges what the status of the scheme request is as below:



= The scheme has been validated as being feasible and is available for consideration



= The scheme has previously been approved for design only/feasibility study and the results are awaited before the scheme can be fully considered



= The scheme request is against ECC criteria.



= A scheme request has been received and is in the initial validation process.

Passenger Transport

Total Value of schemes	£23,500
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Ref	Location	Description	Problem	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
1	Bus stop IM379b Brook Parade - PT	Relocate and re-mark bus cage.	The bus stop flag and pole relocated to accommodate seating - bus cage in incorrect location and needs to be remarked.	Chigwell	Total scheme	LEPP165001	£3,000		G
2	42001003 Stops at Baldwins Hill- PT	Increase hardstand area, provide barriers and general bus stop improvements.	The current hardstand area is too small for pedestrians and not safe.	Loughton	Implementation	LEPP165010	£13,000	Following on from the detailed design.	G
3	Chigwell Rise, Chigwell - PT	For the installation of flag and post by TFL; To install new kerb for new bus stops.	Currently stops are too far away and an additional one is needed	Chigwell	Total scheme	LEPP155004	£7,500	Top up needed to install proposed bus kerbs and existing footway to be resurfaced to new kerb level	G

Public Rights of Way

Total Value of schemes	£199,000
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Ref	Location	Description	Problem	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
1	Byway 61, Willingale - PROW Improvements	Clear the ditches to protect any new surface from water damage. Remove side overgrowth to let air and sunlight onto the byway so it dries out. Improve the vehicle access along the byway.	Unsafe state of the byway for users. Places with deep ruts filled with water.	Ongar	Total scheme	LEPP168001	£39,000		G
2	Bridleway 66 - PROW Improvements	Improvements to the drainage of the surface, ditch clearance and cutting of grips. Overlaying the surface with road planings and compacting firmly.	Boggy, underlating and uneven surface that makes the bridleway a very difficult route to follow and possibly injurious to use to users if not remedied.	Waltham Abbey	Total scheme	LEPP168002	£35,000	Not all of bridleway but approximately less than 600m of total length will require extensive work.	G
3	Bridleway 89 - PROW Improvements	Improvements to the drainage of the surface, ditch clearance and cutting of grips. Overlaying the surface with road planings and compacting firmly.	Boggy, underlating and uneven surface which in part has been due to damage caused by misuse which now makes the bridleway a very difficult route to follow and possibly injurious to users if not remedied.	Waltham Abbey	Total scheme	LEPP168003	£25,000	Not all of bridleway but approximately less than 300m of total length will require extensive work.	G
4	Footpath 3 - PROW Improvements	The embankment to be shored up with concrete bag work then infilling with concrete and planings and compacting firmly.	Path is disappearing into the river. The footpath at present is uneven and difficult in places to negotiate if unsure of footings. In poor light, the issues may be aggravated.	Theydon Bois	Total scheme	LEPP168004	£50,000		G

Public Rights of Way

Total Value of schemes	£199,000
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Ref	Location	Description	Problem	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
5	Byway 21 - PROW Improvements	Undertake improvement works to the drainage and surfacing to enhance accessibility for all users of the byways along the byways.	At present the lane is partly naturally surfaced. Some sections are open and exposed to the elements while the majority is enclosed by trees and hedges. The constant use by motorised vehicles has made a section of the route almost impassable for most parts of the year. The lane becomes highly waterlogged, rutted and boggy and is very difficult to negotiate and dangerous to use by all users with instances of users including horses and four-wheel users becoming stuck	Fyfield	Total scheme	LEPP168005	£50,000		G

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
1	Pyrles Lane, Chester Road, Hillyfields - Implementation of a speed table	Removing the existing material and replacing with new material to current road specification. Remove old warning signs related to the junction and install new 'staggered junction ahead) warning signs on yellow backing board plus new posts along Chester Road and Hillyfields. Install two new grey backed give way regulatory signs and two new grey backed give way 'x' yards ahead warning sign on Pyrles Lane (north and south) with new posts. Take down some of the existing guard railing which is sited within the visibility splays for Pyrles Lane give way position and replace with PG2 type guard railing	Loughton	Implementation		£29,000	Following feasibility study	A
2	All roads leading into and through the forest - Signage review	Signage investigation to direct any 'lost' HGV's drivers to the more suitable roads.	Epping Forest	Feasibility	LEPP162009	£10,000		G
3	B181/Upland Road - Feasibility study for junction improvements	Widening and signage design, including topographical survey and Road Safety Audit.	Epping Upland	Design		£5,000	Following feasibility study	G

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
4	Laundry Lane, Nazeing - Stopping up	To implement a No through Road or install a lockable gate.	Nazeing	Total scheme		£0	Further discussion needed at a Highway Surgery with the Cabinet Member as discussed at the December meeting.	R
5	B181 Lindsey Street, Epping - VAS Sign	Install a VAS by Shaftesbury Farm going west.	Epping	Total scheme		£0	Against Officer recommendation. The speed limit at this location is 30mph. The 7 day average daily speed northwest bound was 35mph and 31.5mph southeast bound. Although the criteria is met northwest bound, drivers will drive at an appropriate speed for the geometry of the road. There are fields on one side of the carriageway and properties are set back from the carriageway. As drivers are leaving Epping they will likely accelerate once passed the majority of houses and this is the reason for the 35mph average.	R
6	Tidy's Lane, Epping - TMI	Feasibility study to look at the options to prevent the verge from being torn up along Tidy's Lane.	Epping	Feasibility	LEPP162027	£3,000		G

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
7	Pump Hill, Loughton - Signage	Install 'give way triangle' to highlight the give way markings.	Loughton	Total scheme	LEPP162062	£3,000	A SLOW marking should only be installed next to a sign so that they work in conjunction, an advance give way warning sign (TSRGD 501) could be installed however as the footway is very narrow and the only suitable location would be on the Street Light column and officers wouldn't recommend this option.	G
8	A113 London road, Stanford Rivers – North of Church road - TMI	Remove the centre lining where the width of the carriageway is less than 5.5m	Stanford Rivers Parish Council	Total scheme	LEPP162033	£0	In validation	V
9	Sedge Green, Roydon - Signage	Request for possible signage- (other danger ahead) with a 'road liable to flood' sub plate	Nazeing	Total scheme	LEPP162036	£0	In validation	V
10	Honey Lane (east of Wood Green Road), Waltham Abbey	Option study to aid pedestrians to cross the road.	Waltham Abbey	Total scheme		£0	Following the feasibility study, there was 3 options. However, 2 will need Cabinet Member Approval as they are against policy and 1 would need significant Corporation of London land.	R
11	Ivy Chimney Road, Epping	A feasibility study to be carried out to determine options to improve the parking issue along the footway.	Epping	Total scheme		£0	Awaiting feasibility study completion.	A

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
12	Highfield's Green J/w Bury Lane- Keep Clear Markings	A Keep Clear box at the junction to help vehicles exit when there is a high volume of traffic	Epping	Total scheme	LEPP162037	£0	During the site visit there was no evidence of queuing across the junction. More evidence needed before action taken.	A
13	Buckhurst Way, Buckhurst Hill	Traffic calming measures	Epping	Total scheme		£0	Awaiting speed survey results.	V
14	Forest Lane, Chigwell	Traffic calming measures	Chigwell	Total scheme		£0	Against policy. The speed limit at this location is 30mph. Following the 7day speed and volume survey, the average daily speed south bound was 28.4mph and 28mph north bound. This would be deemed very good compliance. There have been no accidents along this stretch of road. Survey was carried out from Saturday 7th to Friday 13th January.	R
15	Alderton Hall Lane jw Alderton Hill- Junction protection Markers	Install Double Yellow lines as junction protection markers	Loughton	Total scheme	LEPP162041	£0	Against officers recommendation. No parking was identified on site at the time of the site visit and the site is some distance away from any schools or town centre shops. Furthermore every house at and near the junction had off street parking provision for multiple vehicles. Therefore no on street parking issues were identified or considered to be problematic and no measures are proposed.	R

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
16	Lower Rd/England Lane- junction protection markers	Install double yellow lines as junction protection markers	Loughton	Total scheme	LEPP162042	£0	Against officers recommendation. All properties near to the junction had off street parking provision and there are no properties located directly at the junction. No parking was observed at the time of the site visit.	R
17	St Johns Rd/A121 both sides- Junction protection markers	Extend the double yellow lines by 15m due to new housing estate entrance being blocked by vehicles	Loughton	Total scheme	LEPP162043	£0	Against officers recommendation. Double yellow lining are already implemented throughout the A121 junctions with Church Lane and St Johns Road. These extend around 15m into both junctions and would eradicate any visibility / safety concerns.	R
18	The Drive/Habgood	Replace the single yellow line with double yellow lines for junction protection.	Loughton	Total scheme	LEPP162044	£4,500	This will include the design, Traffic Regulation Order and works/Supervision elements.	G
19	The Uplands, jct with entrance to Cricket Club	Install double yellow lines to reduce the parking on the bend of the Cricket Club entrance	Loughton	Total scheme	LEPP162045	£0	Against officers recommendation. If cars are parked over the dropped crossing this would be an enforcement issue.	R
20	The Campus jw Boarders Lane	Double Yellow Lines to be extended approximately 15m into 'The Campus' and beyond the existing pedestrian refuge islands on both sides of the junction	Loughton	Total scheme	LEPP162046	£4,500	This will include the design, Traffic Regulation Order and works/Supervision elements.	G
21	Brooklyn Av jw Churchfields	Replace the single yellow line with 15m double yellow lines at the junction	Loughton	Total scheme	LEPP162047	£4,500	This will include the design, Traffic Regulation Order and works/Supervision elements.	G

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
22	Burney Drive/Colebrook Lane	To implement double yellow lines for a distance of approximately 15m from the junction.	Loughton	Total scheme	LEPP162048	£4,500	This will include the design, Traffic Regulation Order and works/Supervision elements.	G
23	Colebrooke jw Colebrooke Gardens	To implement double yellow lines for a distance of approximately 15m from the junction.	Loughton	Total scheme	LEPP162049	£4,500	This will include the design, Traffic Regulation Order and works/Supervision elements.	G
24	Traps Hill, South side, entrance road to surgery	Install double yellow lines to improve sight lines of vehicles exiting the surgery	Loughton	Total scheme	LEPP162050	£0	Officers would not agree with installing junction protection markings at this location or the installation of the double yellow lines from this point to the A121. We do not install junction protection markings for individual drives/properties.	R
25	Staples Road j/w Shaftesbury, Loughton	Install a salt bin	Loughton	Total scheme	LEPP162051	£500		G
26	Fyfield Road, Ongar	Upgrade the existing zebra crossing	Chipping Ongar	Total scheme		£0	Against policy. The speed limit at this location is 30mph. The Pedestrian/vehicle (PV2) survey over 7 days was 0.100. Where the value of a PV2 is below 0.2 then a signalised crossing point would not be justified.	R

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
27	Coopers Hill, Ongar - between Brentwood Road and The Borough	Traffic calming measure along this section of Coopers Hill	Ongar	Total scheme		£0	Against policy. The speed limit at this location is 30mph. Following two 7day speed and volume surveys, the average daily speed South bound range from 28.8mph and 31.3mph and north bound range from 28.7mph and 30.6mph This would be deemed very good compliance.	R
28	Fairmead Rd at Crossroad to Robin Hood Roundabout	Install Horse Warning Signs	Waltham Abbey	Total scheme	LEPP162053	£0	In validation.	V
29	Wake Road j/w Woodriddern Hill, Waltham Abbey	Install Horse Warning Signs	Waltham Abbey	Total scheme	LEPP162054	£0	In validation.	V
30	Avey Lane, Waltham Abbey	Install Horse Warning Signs	Waltham Abbey	Total scheme	LEPP162055	£0	In validation.	V
31	Mott Street, Waltham Abbey	Install Horse Warning Signs	Waltham Abbey	Total scheme	LEPP162056	£0	In validation.	V
32	Daws Hill and Bury Road, Waltham Abbey	Install Horse Warning Signs	Waltham Abbey	Total scheme	LEPP162057	£0	In validation.	V

Traffic Management

Total Value of schemes	£88,000
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
33	Mott Street, Waltham Abbey	Advisory '20mph when lights show' sign and wigwags on both approaches to the Primary School.	Waltham Abbey	Total scheme	LEPP162058	£0	In validation.	V
34	HGV Routing for Lea Valley Glasshouses	To review HGV movements in the vicinity of the glasshouses industry in Roydon.	Roydon and surrounding villages	Studies	LEPP162064	£15,000	This study will identify existing HGV trips through the study area, investigate what travel information is given to HGV drivers and how this can be improved. In addition to routing strategies, the study will also look into weight restrictions, existing signing and HGV routing strategies and how these can be improved.	G
35	Pick Hill, Waltham Abbey	Investigate if Pick Hill can be made a one way	Waltham Abbey	Total scheme	LEPP162065	£0	Awaiting survey result	V

Walking

Total Value of schemes	£0
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Ref	Location	Description	Parish	Scheme stage	Cost Code	Estimated cost	Comments	RAG
1	Church Road, Moreton - Footway	Installation of a footway for 32m outside Moreton Primary School.	Moreton	Implementation		TBC	The implementation cost is around £20k, however we will need to relocate GigaClear Fibre Optics and until the relocation costs are provided we will not have an estimate for the total cost I will advise accordingly.	G
2	Church Lane to The Rabbits bus stop-Footway	Request for a footpath to make access to The Rabbits bus stop easier for residents that live down Church Lane	Stapleford Abbots	Total scheme	LEPP163001	£0	In validation.	V

Epping Forest 2016/17 S106 programme

Current Stage	Task Name	Cost Code	Delivery Method	Works Description	Comments
In Progress	CHURCH HILL - ZEBRA EPF1267	HB13002	Supply Chain Partner	New zebra crossing, Church Hill Loughton	Scheme programmed to start Quarter 1
In Progress	MILL LANE TRAFF CALM	HB15007	Supply Chain Partner	VAS to be installed	Programmed for delivery Quarter 1

The 2017/18 programme is yet to be finalised, but will be presented at the next LHP meeting

Rangers Report

Defect Date	Area Name	Site Name	Defect Location	Defect Description	Feature Type by Name	Status	Date Attended
02/12/2016 13:07	Roydon	HIGH STREET	O/s RHB bar near j/w Epping Road	RA 3=3x1. Weight limit signs on lit post S3 misaligned-----Rangers	PR2 Road	Completed	06/12/2016
06/12/2016 09:01	Chigwell	LAMBOURNE ROAD	Opp lamp column no 33	Ra3 1x3 road sign dirty rangers work	PR2 Road	Completed	07/12/2016
07/12/2016 14:50	Nazeing	MIDDLE STREET	O/s Green Elms approx 30m from j/w Perry Hill	RA 4=2x2. Bus stop post and sign damaged/k/d. Bus Stop code: esxgmwja-----GCNE. -----Rangers	PR2 Road	Completed	08/12/2016
13/12/2016 09:18	Stapleford Abbotts	OAK HILL ROAD	Opp tysea hill	Ra3 1x3 finger post dirty. Rangers work	PR2 Road	Completed	14/12/2016
13/12/2016 12:45	Waltham Abbey	EPPING ROAD	Opp Shell Garage	RA 3=3x1. 3m of missing/damaged Pcc kerb----Rangers	PR1 Road	Completed	14/12/2016
13/12/2016 12:59	Stapleford Abbotts	OAK HILL ROAD	Os travern	Ra4 2x2 surface cover fillet missing around cover 0.6 m x 0.6 m x 40 mm, Rangers work	PR2 Road	Completed	14/12/2016
19/12/2016 00:00	Stapleford Abbotts	footpath 22 Stapleford Abbotts	From woodlands farm to oakfields	Ra4 1x4 veg in fway approx 100 m Rangers work	footpath	Completed	20/12/2016
19/12/2016 08:02	Stapleford Abbotts	footpath 22 Stapleford Abbotts	From highhouse to woodlands farm veg in fway approx 100 m Rangers work	Ra4 1x4 veg in fway approx 100 m Rangers work	footpath	Completed	20/12/2016
19/12/2016 08:07	Stapleford Abbotts	footpath 22 Stapleford Abbotts	From high house to grove house	Ra4 1x4 veg in fway 100 m Rangers work	footpath	Completed	20/12/2016
19/12/2016 09:04	Stapleford Abbotts	STAPLEFORD ROAD	Side of chase lodge	Ra3 1x3 keep left signs dirty Rangers work	PR2 Road	Completed	20/12/2016
19/12/2016 09:24	Stapleford Tawney	PASSINGFORD BRIDGE ROUNDABOUT	Jun stapleford rd	Ra3 1x3 arrow sign dirty Rangers work	PR1 Road	Completed	21/12/2016
19/12/2016 10:05	Roydon	TYLERS ROAD	Approx 20m from j/w Epping Road on l/c 1	RA 2=2x1. Weight restriction information plate misaligned on l/c----Rangers.	PR2 Road	Completed	21/12/2016
19/12/2016 10:46	Nazeing	SEDGE GREEN	Opp The Willows approx 20m from Pecks Hill	RA 2=2x1. Speed signs misaligned on post-----Rangers	PR2 Road	Completed	21/12/2016
20/12/2016 08:20	Chigwell	ROMFORD ROAD	On lamp column no 6	Ra3 1x3 sign dirty no u turn Rangers work	PR1 Road	Completed	21/12/2016
20/12/2016 08:41	Chigwell	WOOLHAMPTON WAY	Os 2	Ra3 1x3 road sign dirty Rangers work	Local Road	Completed	22/12/2016
20/12/2016 10:11	Chigwell	CHIGWELL RISE	Os 1	Ra3 1x3 road sign dirty Rangers work	PR2 Road	Completed	22/12/2016

Rangers Report

20/12/2016 12:18	Chigwell	PUDDING LANE	Approx 50 m from abridge rd	Ra3 1x3 road sign dirty Rangers work	Local Road	Completed	22/12/2016
20/12/2016 12:22	Chigwell	PUDDING LANE	Approx 75 m from abridge rd	Ra3 1x3 road sign dirty Rangers work	Local Road	Completed	23/12/2016
20/12/2016 12:51	Lambourne	NEW ROAD	Jun manor rd	Ra3 1x3 road sign dirty Rangers work	Local Road	Completed	23/12/2016
20/12/2016 13:32	Stanford Rivers	SCHOOL ROAD	Os golf course	Ra3 1x3 road sign faded double bend Rangers work	PR2 Road	Completed	26/12/2016
20/12/2016 13:34	Stanford Rivers	SCHOOL ROAD	Opp golf couse	Ra3 1x3 road sign faded 30 mph Rangers work	PR2 Road	Completed	26/12/2016
20/12/2016 13:36	Stanford Rivers	SCHOOL ROAD	Os golf course	Ra3 1x3 road sign faded national speed Rangers work	PR2 Road	Completed	26/12/2016
20/12/2016 13:38	Stanford Rivers	SCHOOL ROAD	Opp golf course	Ra3 1x3 road sign faded national speed Rangers work	PR2 Road	Completed	26/12/2016
20/12/2016 14:19	Chigwell	HIGH ROAD	Os 40	Ra3 1x3 road sign dirty , side rd left Rangers work	PR1 Road	Completed	27/12/2016
20/12/2016 15:08	Roydon	EPPING ROAD	j/w common road	RA 2=2x1. 4no dirty signs-----rangers	PR1 Road	Completed	27/12/2016
21/12/2016 12:34	Chigwell	FOREST LANE	Jun manor rd	Ra4 1x4 give way sign dirty Rangers work	PR2 Road	Completed	27/12/2016
22/12/2016 08:37	Stapleford Abbotts	STAPLEFORD ROAD	Jun with roundabout	Ra3 1x3 sign dirty Rangers work	PR2 Road	Completed	28/12/2016
22/12/2016 10:54	Chigwell	RODING LANE	Jun high rd	Ra3 1x3 give way sign dirty Rangers work	Local Road	Completed	28/12/2016
23/12/2016 12:01	Chigwell	CHIGWELL ROAD	Os jolly wheelers pub	Ra3 1x3 weight limit sign dirty Rangers work	PR1 Road	Completed	28/12/2016
03/01/2017 11:33	Chigwell	ROMFORD ROAD	Opp lamp column no 2	Ra4 1x4 40 mph sign dirty Rangers work	PR1 Road	Completed	04/01/2017
03/01/2017 11:33	Chigwell	ROMFORD ROAD	Opp lamp column no 2	Ra4 1x4 40 mph sign dirty Rangers work	PR1 Road	Completed	05/01/2017
09/01/2017 10:13	Waltham Abbey	CHURCH STREET	O/s 3-4 on vxo	RA 3=3x1. Loose/rocking Pcc kerb----Rangers	Local Road	Completed	11/01/2017
10/01/2017 10:08	Stapleford Tawney	PASSINGFORD BRIDGE ROUNDABOUT	Jun ongar rd	Ra3 1x3 road sign dirty Rangers work	PR1 Road	Completed	11/01/2017
11/01/2017 12:09	Waltham Abbey	BROOKER ROAD	Opp Pad Contracts	RA 6=3x2. Bollard damaged/broken leaving 80mm upstand in f/w-----Rangers	Local Road	Completed	12/01/2017
16/01/2017 13:55	Waltham Abbey	EPPING ROAD	O/s Miller and Carter by l/c 2	RA 1=1x1. Wooden post and 40mph sign misaligned/leaning----Rangers	PR1 Road	Completed	17/01/2017

Rangers Report

19/01/2017 12:22	Waltham Abbey	WOODGREEN ROAD	S/o 33 by j/w Honeypot Lane	RA 3=3x1. K/d/leaning post and signs----Rangers----GCNE	Local Road	Completed	20/01/2017
20/01/2017 14:00	Waltham Abbey	SILVER STREET	J/w Paradise Road on lit post S3	RA 3=3x1. No Entry Sign Misaligned on post----Rangers.	Local Road	Completed	20/01/2017
08/02/2017 14:50	Waltham Abbey	HILLHOUSE	O/s Abbey news by Bt cover	RA 2=2x2. F/w defect 1m x 0.1m x 25mm----Rangers	Local Road		
08/02/2017 14:50	Waltham Abbey	HILLHOUSE	O/s Mann bros by Bt cover	RA 4=2x2. F/w defect/upstand to cover 0.6m x 0.1m x 20mm---Rangers	Local Road		
08/02/2017 14:51	Waltham Abbey	HILLHOUSE	R/o Community centre by Aerial mast	RA 4=2x2. Misaligned f/w cover by 20mm---Rangers	Local Road		
13/02/2017 13:28	Waltham Abbey	WOOLLARD STREET	O/s 15	RA 3=3x1. Plastic bollard fixing damaged so can be removed without using key----Rangers.	Local Road		
13/02/2017 13:52	Waltham Abbey	ROUNDHILLS	Opp 153 by garages approx 30m from j/w Downlands	RA 4=4x1. Concrete bollard loose/leaning----Rangers.	Local Road		
14/02/2017 09:25	Chigwell	MANOR ROAD	From Opp 209 to Opp no 231	Ra3 1x3 fw requires restablishing sidings approx 100 m. Rangers work	PR2 Road		
20/02/2017 09:53	Lambourne	LONDON ROAD	Opp the chase	Ra4 1x4 2x 40mph repeater sign dirty. Rangers work	PR1 Road		
20/02/2017 14:41	Waltham Abbey	BLACKMORE COURT	R/o 4 Blackmore Court	RA 3=3x1. F/w defect/misaligned/rocking modular paving slabs by 4 x 30mm--Rangers	Remote Footway (ws)		
20/02/2017 14:41	Waltham Abbey	BLACKMORE COURT	r/o 1-7 Blackmore court	RA 2=2x1. 10no broken/uneven slabs x 20-25mm-----poss rangers	Remote Footway (ws)		
20/02/2017 14:42	Waltham Abbey	BLACKMORE COURT	From r/o 7 Blackmore court to no 8	RA 3=3x1. F/w defect 5 no misaligned/broken slabs x 25mm. Rangers	Remote Footway (ws)		
22/02/2017 13:45	Waltham Abbey	WINTERS WAY	O/s 35 by BT cover	RA 4=1x4. Misaligned Modular Paving slab and damaged cement fillet approx 0.6m x 0.1m x 20mm----Rangers	Local Road		

APPENDIX

1. Honey Pot Lane Feasibility Study
2. B181 Epping Road junction with Upland Road Technical Advice Note
3. B181 Epping Upland Design 1
4. Epping Upland Design 2

Feasibility Study

DS5149 Honey Lane, Waltham Abbey – Pedestrian Crossing

Job Number:	DS5149
Doc Ref:	Feasibility Study
Author:	James O'Regan

Document History

Revision	Purpose	Originated	Checked	Approved	Date
N/A	Issue	JO	BSE	JO	3/3/17

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2.	<i>Existing Site Conditions</i>	3
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1. Introduction

1.1 *Project Background*

It has been requested that Essex Highways look into the implementation of a pedestrian crossing along A121 Honey Lane in Waltham Abbey. The Epping Forest Local Highways Panel has supported the request for this scheme.

Taken from the Scheme Validation:

“Residents from a nearby housing estate (to the north of Honey Lane and the east of Wood Green Road) have raised concerns that they cannot cross the A121 Honey Lane due to the high speeds and volumes of traffic encountered along the road.”

The aim of this feasibility study is to assess the suitability of a pedestrian crossing along Honey Lane. The section under investigation is between Woodgreen Road and Woodbine Close.

2. Existing Conditions

2.1 *Location / Land Use*

- A121 Honey Lane is a Priority Route 1 that is situated close to Waltham Abbey in the district of Epping Forest.
- The A121 Honey Lane is heavily trafficked as it connects with the M25 and Waltham Abbey to the west and to the Wake Arms roundabout in the east, which then provides direct routes to Epping, Loughton and Chigwell.
- A caravan park estate is situated directly to the north, accessed by Woodgreen Road and Woodbine Close.
- Epping Forest Conservators Land is situated directly to the south and a Jewish Cemetery situated to the south west. An open field and a car park flank the highway directly to the south in the area under investigation.
- A121 Honey Lane is a bus route and bus stops are located close to the Woodgreen Road junction for both directions.
- The Woodbine Inn Public House is situated on the northern side of the highway to the east of Woodgreen Road. A small number of residential properties are also situated beyond the northern side of the highway, either side of Woodbine Close.
- The speed limit is 40mph along the section of Honey Lane under investigation.

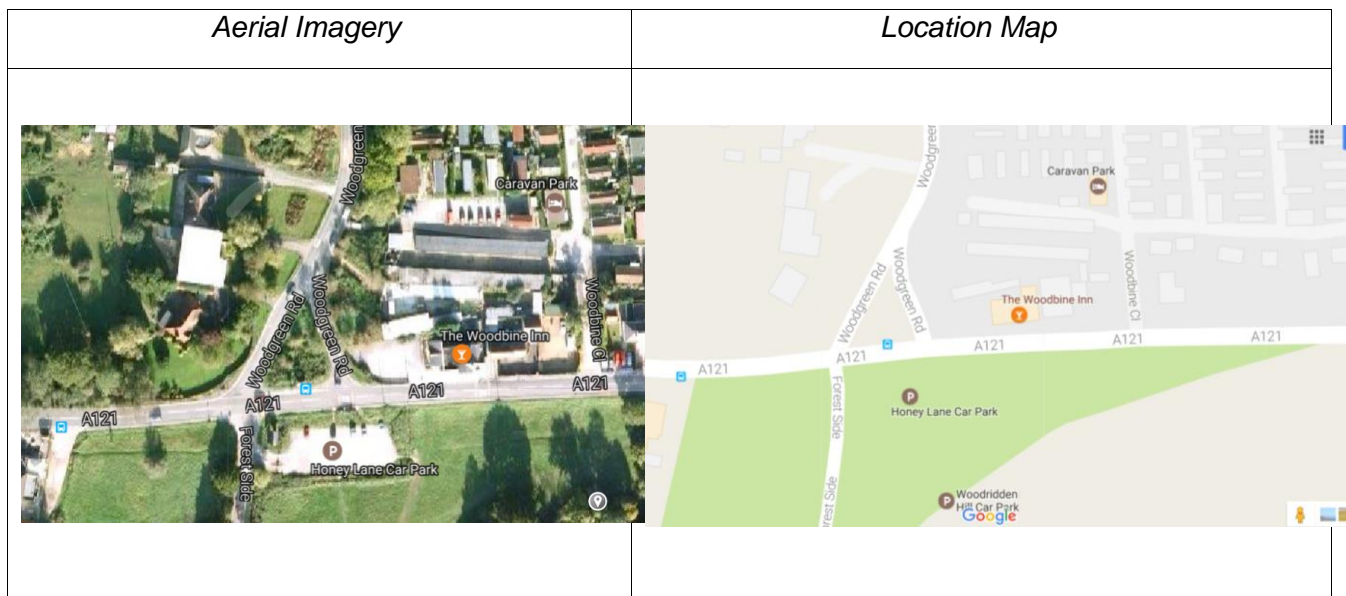


Fig 1.1 the location map and aerial imagery for Honey Lane, Waltham Abbey.

2.2 Site Observations

A site assessment was undertaken on the 7th February 2017, a summary of the main findings are below:

- Existing carriageway and footway widths are narrow along this section of Honey Lane. The footway on the southern side of the highway is 1.4m and the carriageway is 6.3m. The footway was measured at 1.2m on the northern side of the highway to the west of Woodgreen Road. No footway exists outside of the Woodbine pub. The northern footway to the east of the pub was measured at 1.1m. These measurements are shown on drawing DS5149-00-001.
- Vehicle speeds were high and flows were constant. There was little sign of pedestrian crossing activity.
- Street lighting exists to the west of Woodgreen Road but none is present to the east of Woodgreen Road.
- A central line demarcation roadmarking exists along with edge of carriageway markings. A number of 'SLOW' roadmarkings are present.
- Yellow backed horse warning signage is present for both directions. Additional crossroads ahead and elderly pedestrian yellow backed warning signage exists for westbound traffic.

2.3 Highway Boundary

A Highway Boundary check was requested and the results are shown below in Fig 2.1.

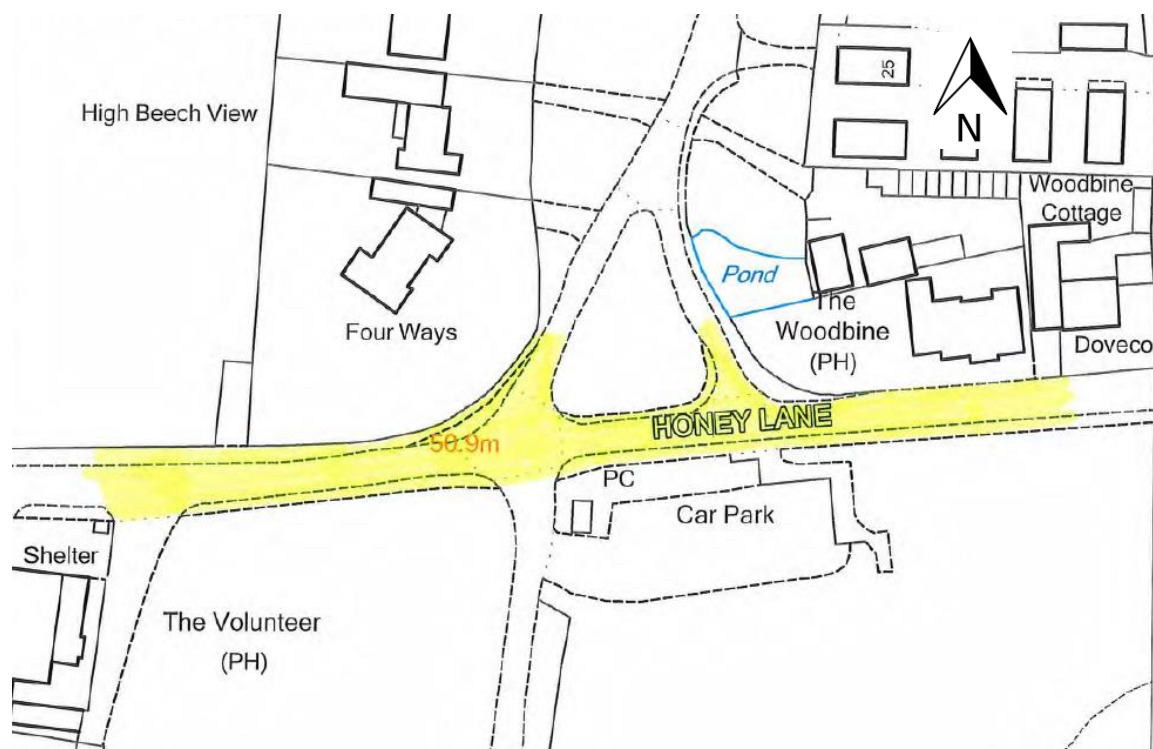


Figure 2.1: Highway Boundary Plan

The area highlighted in yellow is considered to form part of the publicly maintainable highway.

2.4 Collision Data

There have been 5 Personal Injury Collisions (PICs) recorded within the area under investigation between 31st Aug 2013 and 31st Aug 2016. All of these collisions resulted in 'slight' injuries. Four of these collisions involved motorised vehicles only. One of the collisions also involved a cyclist. None of the collisions involved pedestrians. Collision statistics have been sourced from Essex County Council's Traffweb Information Database and include all reportable collisions involving personal injury. The location of collisions are shown below.

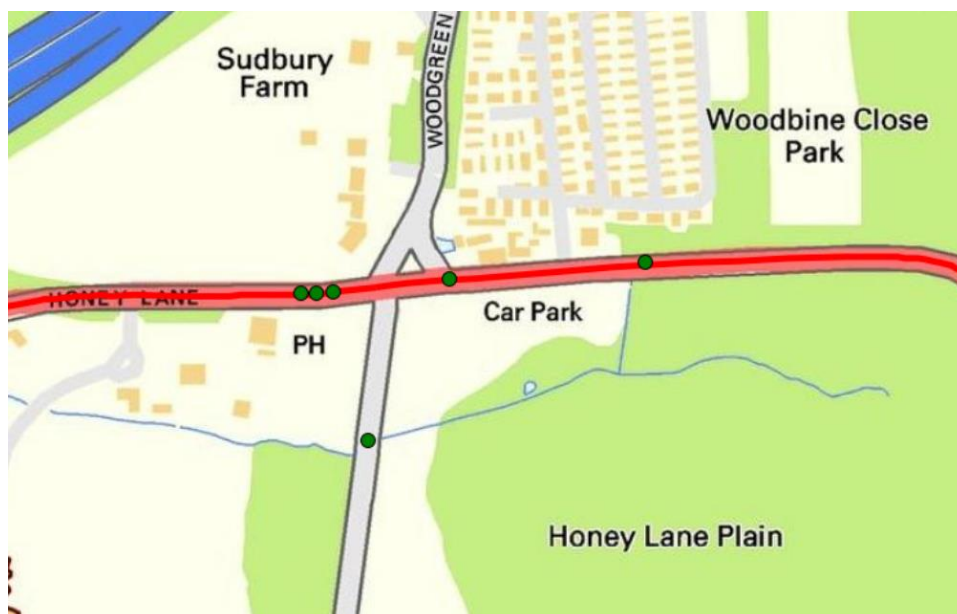


Figure 2.3: PICs Location Plan along A121 Honey Lane

2.5 Statutory Undertakers Plant

As part of the investigation into the feasibility of this project, a statutory undertaker's plant request was made which highlighted the following:

- *A low pressure gas main appears to be present running along the length of the southern footway in an east to west direction, depth unknown.*
- *A 3 inch Cast Iron Water mains runs along the existing carriageway in an east to west direction, depth unknown.*
- *A foul water sewer runs along the existing carriageway in an east to west direction, at a depth of approximately 2 to 2.5 metres.*
- *BT Open Reach plant runs along the existing carriageway in an east to west direction, depth unknown.*

- *Low voltage electrical cables run along the northern footway in an east to west direction, depth unknown.*

2.6 Speed Survey Data

A seven day automatic traffic count (ATC) and speed survey was undertaken on Honey Lane between Tuesday 23 June and Monday 29 June 2015. The results of the survey showed

- Eastbound mean speeds of 36.1mph, eastbound 85th percentile speeds of 40.3mph.
- Westbound mean speeds of 35.8mph, westbound 85th percentile speeds of 41.2mph.
- Combined mean speeds of 36.0mph and combined 85th percentile speeds of 40.7mph

Further data is shown as Appendix A. The results are summarised below:

	Combined	Eastbound	Westbound
85 th percentile speeds	40.7 mph	40.3 mph	41.2mph
Mean speeds	36 mph	36.1 mph	35.8 mph

2.7 Pedestrian Survey Data

A PV² calculation was undertaken using data from the ATC and a further manual classified pedestrian crossing count undertaken on Tuesday 23rd June 2015. The PV² calculation is the standard calculation for determining the need to install a pedestrian crossing. It calculates need by factoring traffic volumes from the four busiest hours and then squaring this value. This figure is then multiplied by the corresponding factored pedestrian count, then divided by 10⁸ to produce an hourly PV².

The collision factor multiplied by the difficulty factor, further multiplied by the original PV² value, provides the CDPV². Where the value of CDPV² is between 0.2 and 0.7 x 10⁸, then a signal controlled crossing would not be recommended, and alternatives such as a pedestrian refuge or zebra crossing should be considered. Where the value of CDPV² is below 0.2, then a crossing facility would not normally be justified. The site however, may be reviewed on its merits with regard to local and/or special needs and may be considered subject to approval of a CMA (Cabinet Members Action).

The CDPV² for this location equates to 0.205x10⁸. This value is sufficient to justify a zebra crossing or pedestrian refuge island, but clearly, it is at the lower end of the

bandwidth. The value falls someway short of what would be required to justify a signalised pedestrian crossing. This data is included as Appendix A.

2.8 Photographs

Honey Lane looking west from northern footway



Honey Lane looking west from southern footway



Honey Lane looking east from northern footway



Honey Lane looking east from southern footway



3. Design Options

3.1 Option 1 – Zebra Crossing

The first option that was considered was to introduce a zebra crossing. Although the CDPV² value is sufficient to suggest that a zebra crossing could be introduced best practice guidance states that a zebra crossing could not be introduced on safety grounds.

Local Transport Note (LTN) 1/95, the overarching guidance for the assessment of pedestrian crossings states that 'Zebra crossings should not be installed on roads with an 85 percentile speed of 35 m.p.h. or above.'

The speed survey data shows 85th percentile speeds over 5mph above what is considered acceptable. Therefore the implementation of a zebra crossing facility is not possible on safety grounds.

3.2 Option 2 – Signal Controlled Pedestrian Crossing

The second option to be considered was a signal controlled crossing point. A signal controlled crossing is not constrained by 85th percentile speeds but falls somewhat short of the required CDPV² for a signal controlled crossing. The required value for a signal controlled crossing is 0.7×10^8 . The current value is 0.205×10^8 .

The introduction of a signal controlled crossing facility would therefore be outside of policy. A CMA (Cabinet Members Action) could be signed to allow for the provision of a signal controlled crossing. This would not be recommended by Officers as the CDPV² is so low. Additionally there are no other considerable factors that would add further weight to the introduction of such a facility – There are no collisions involving pedestrians and there is no evidence of community severance.

3.2 Option 3 – Pedestrian Refuge Island

The final option to be considered was the introduction of a pedestrian refuge island. As with a zebra crossing a pedestrian refuge island can be introduced if the CDPV² is greater than 0.2×10^8 and for this location it is at 0.205×10^8 . There is also no prohibition on speed values. Site assessments do show however that a pedestrian refuge island cannot be safely accommodated given the existing highway width available.

The minimum permitted footway width according to 'Designing for Pedestrians – ECC Guide to Good Practice' is 1.5 metres. The site measurements show that existing footway widths fall beyond this at 1.1/1.2m and 1.4m. Furthermore ECC guidance also states that a pedestrian refuge island should be at least 1.5m in width and that the adjacent traffic lane widths for 40mph roads should be 3.9m wide or more.

The existing footway and carriageway widths therefore fall drastically short of what would be required to safely accommodate a pedestrian refuge island here. For a pedestrian refuge island to be safely accommodated a total highway width of 12.5m is required (3.9m+3.9m+1.5m+1.5m+1.5m). The existing widths of 8.8/8.9m fall drastically short of this.

From observation of the highway boundary plan it appears that 8.8m is the total available highway width as well. Increasing any highway space to the north would not be possible as the highway directly flanks a number of private residences and the Woodbine Inn Public House. Widening to the south would also appear unfeasible as the highway is directly flanked by Epping Forest Conservators Land.

This option has therefore not been pursued any further. If an area of Conservators was purchased / dedicated to the highway it would be possible to further investigate the scheme. The area required would be of 4m in width and would need to extend to around 100m either side of any proposed crossing point to allow for a suitable inter-visibility zone. The most suitable location of any crossing point would be just to the west of Woodbine Close.

It should also be noted that the introduction of any such facility may also involve the diversion of statutory undertakers' plant as the existing carriageway would need to be realigned. Costs have not been sought at this stage but could be considerable.

4. Recommendation

The pedestrian survey and speed survey data that was collected highlights that no pedestrian crossing facility is suitable or possible owing to safety requirements, policy or available highway land.

A zebra crossing would not be advisable as existing speeds would result in safety concerns that cannot be mitigated. A signal controlled crossing does not meet policy and a pedestrian refuge island cannot be accommodated given the available highway space.

If a CMA against policy is signed to allow a signal controlled crossing then this can progress to design stage. It is recommended that the ECC ITS team is engaged at an early stage however, who will be able to advise on any issues that would affect the feasibility of a signal controlled facility such as required footway widths for signal poles to be introduced. Similarly if Epping Forest Conservators Land can be

purchased to the area outlined above then a refuge island scheme can progress to design stage. Costs have not been provided at this stage but could be considerable owing to the relocation of utility services that may be required.

Technical Advice Note

Proposed change to the priority at B181 Epping Road junction with Upland Road

Job Number:	DC1785
Doc Ref:	Technical Advice
Author:	Fadi Jereis

Document History

Revision	Purpose	Originated	Checked	Approved	Date
N/A	Draft for approval	FJ	SM	CB	
		02/07/2014	15/08/2014	15/08/2014	
A	Addendum	JO	SM	JO	07/03/2017

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1. Introduction

1.1. *Project Background*

Funding of £2,000 was approved by the Epping Local Highways Panel (LHP) to undertake a feasibility study on the proposal of changing the priority of the B181 at its junction with Upland Road.

The request originated from Councillor Anthony Jackson through Epping LHP to change the priority of the B181 junction with Upland Road. By removing the give way sign on the B181 and installing it on Upland Road, potentially this will improve traffic flow along the B181, but the traffic coming from Upland Road to B181 will be required to give way before joining the traffic at the B181. The scheme was approved and is being funded by Epping LHP.

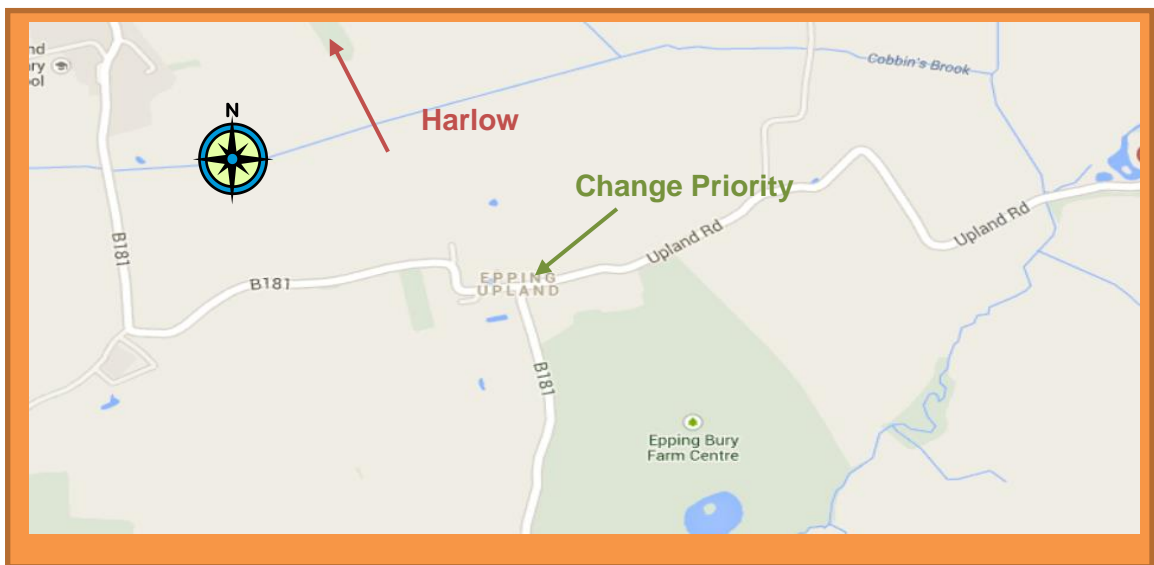
Highways Improvement Design Team are required to:

- Consult with Network Management team to validate scheme proposal.
- Review existing B181 Junction with Upland Road and assess proposed priority change to the B181 Junction with Upland Road and junction impact.
- Subject to Network Management approval review of the existing junction layout to be carried out and proposed changes to the junction layout to be designed.
- Prepare design drawings and target cost
- Arrange Road Safety Audit assessments
- Consult with affected frontages, Essex Police, Free Transport Association and others as appropriate.
- Update Highways Liaison Officer (HLO), Epping Parish Council and LHP as appropriate
- Produce a feasibility report

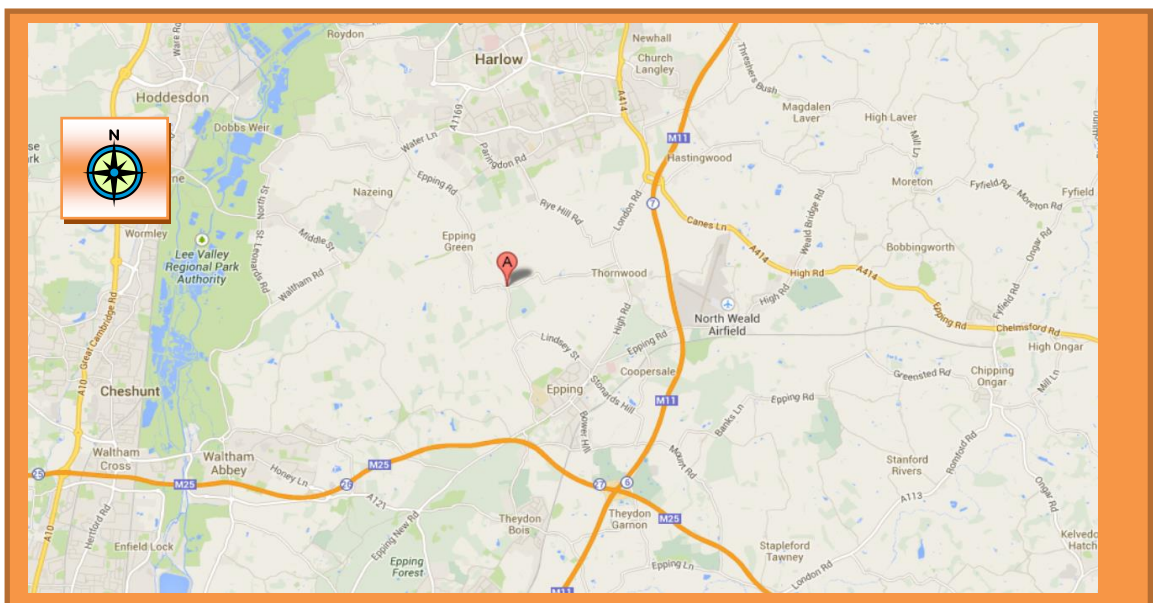
2. Existing Conditions

2.1 Location

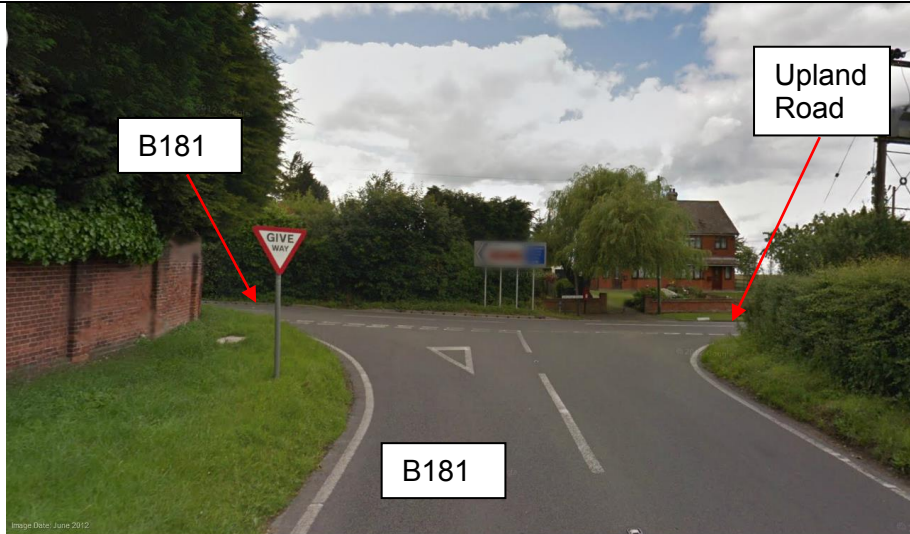
- Epping Upland is a village situated in the Epping Forest district. It lies north of Epping and south of Harlow.
- B181 Junction with Upland Road at Epping Upland Village.



Site location plan : Not To Scale



Epping Upland location : Not To Scale

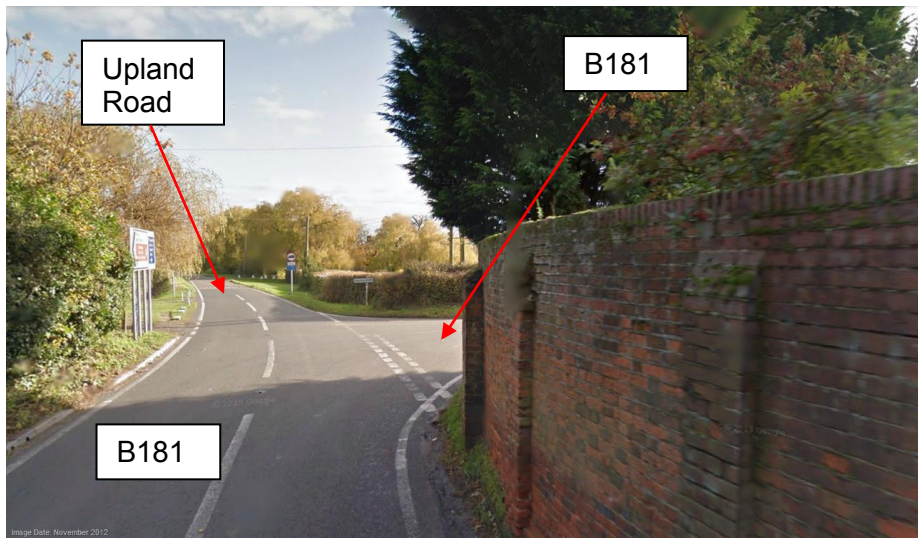


B181

Upland Road

B181

Looking north at T junction of B181 with Upland Road

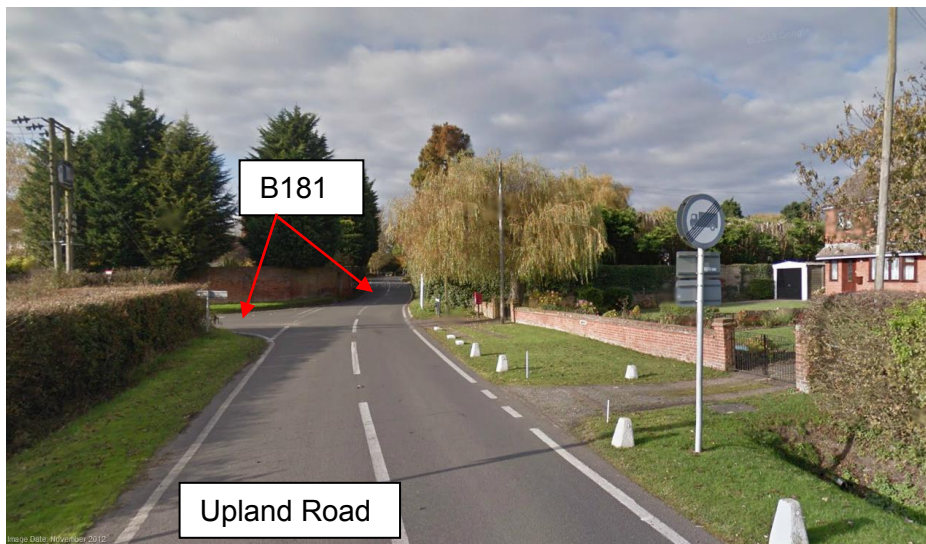


Upland Road

B181

B181

Looking East at T junction of B181 with Upland Road



B181

Upland Road

Looking West at T junction of B181 with Upland Road

2.2 *Land Use*

- The land on both sides of B181 at Epping Upland belongs to private owners and if the scheme is to progress it will be necessary for Essex to purchase part of the land to be able to carry out the work.

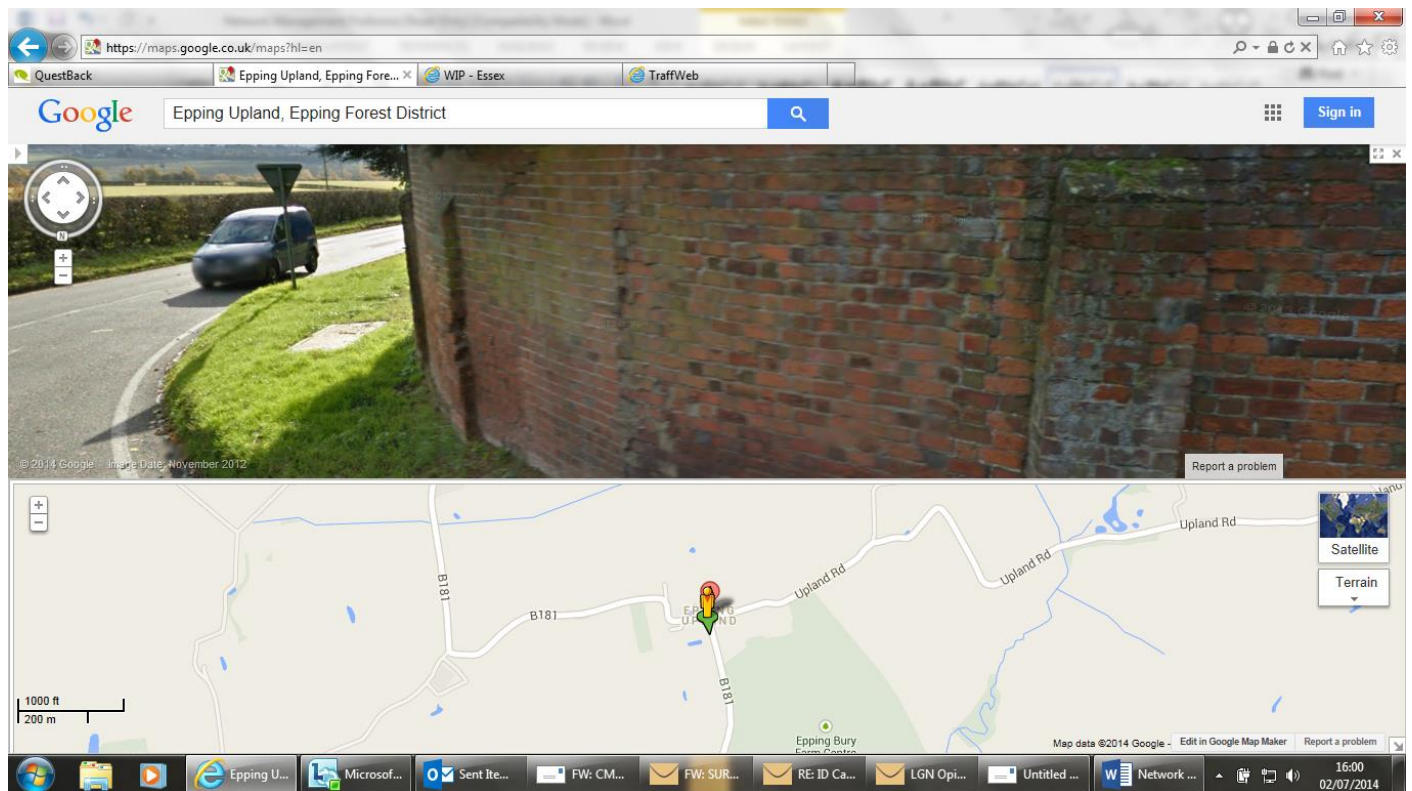
3. Scheme Consultation

3.1 Network Assurance Comments following consultation 02/07/14

Comments received 2nd July 2014 –

Whilst the rationale for changing the priority at this junction appears to have some logic due to the fact that the “T” is formed by the B181 which is the priority route several major issues are apparent that move this into a more major scheme.

The layout change would result in the introduction of a 90^o bend a sever hazard on the PR 2 network. The scheme would require a significant introduction of hazard signs to warn of the bend and the need to reduce speed maximum I would guess to 20 mph. The layout suggests that two HGV’s would not make the turn and one would struggle to align creating another hazard not previously in place. The area is close to what looks like a listed wall and residential housing the probability of these been hit by errant vehicles would be significantly increased. Land will be required in order to realign the junction and urbanisation of the area to accommodate the hazard. The residents should be consulted as the area will be fundamentally changed



A full junction count is required to give a true indication of the queue lengths and to confirm if the problem is justified

The history of the site has not been identified but there are suspicions that this may be an Accident Reduction 2000 scheme. A detailed safety Audit should be undertaken if this is to proceed

3.2 *Road Safety comment received 10/07/14*

The proposals subject to this audit involve relocation of the existing give-way from B181 Epping Road northbound to Upland Road westbound with associated upright sign to diagram 602 'Give-way' sign; and widening of Upland Road at the bell-mouth to create a left and right turn lane at the junction.

During the site visit, the general operational safety of the junction was raised with regard to its current layout. Subsequently, a study of the five-year collision history ending 30th June 2014 at the junction showed there have been only three Personal Injury Collisions (PICs). A brief summary of the collisions is given below:

- 04/10 - Vehicle 1 turns right into B181 Epping Road across the path of a westbound vehicle.
- 03/11 - Vehicle loses control on icy patch of carriageway turning into the junction. Following vehicles also lose control on icy patch and collide into the first vehicle.
- 09/13 - Vehicle 2 waiting at the give-way line on B181 Epping Road to turn right when the front of motorcycle is clipped by vehicle 1 turning right into the junction.

Following a review of the collisions, which shows no dominant pattern, and observation of vehicle movements on site, the Audit Team considers the existing layout to be more suitable than the proposed alignment. Road users travelling northbound on B181 Epping Road must give-way due to poor visibility to the left before emerging on to B181 Epping Road/Upland Road and this manoeuvre was assessed to be generally reducing the risk of a collision at the junction. The scenario only appears to work because road users are forced to come to a stop before undertaking the left turn manoeuvre which reduces over-run of vehicles turning left and increases intervisibility between vehicles turning left on to B181 Epping Road and eastbound vehicles. It is therefore considered that the proposed alignment may result in a severe detrimental impact on the collision record unless significant improvements are made to the layout of the junction including widening of the western arm.

PROBLEM 1

Location(s): B181 Epping Road proposed through route – westbound exit side of bend

Summary: Increased risk of a head on collision

The proposals include changing the junction right of way so that road users travelling south to west along the B181 (and vice versa) have priority over vehicles on Upland Road. During the site visit, it was noted that vehicles which were turning left from the existing give-way were over-running the opposing (eastbound) lane. Most vehicles did this even though they were emerging from the junction at a very slow speed or even starting from a stationary position. If this approach has priority it will result in vehicles travelling at greater speed and over-running the opposing traffic lane more so than with the existing layout. This therefore could significantly increase the risk of a head on collision at this location, particularly involving larger vehicles.

Swept path drawings have been provided for audit. These show that vehicles turning left at the junction take up almost the entire width of the 4.9m carriageway in order to complete the manoeuvre. It is noted that vehicle turning circles will also increase with speed therefore this is likely to constitute a serious hazard.

PROBLEM 2

Location(s): Proposed give-way line at Upland Road

Summary: Increase risk of failure to give-way type collision

During the site visit it was noted that there was dense vegetation along the eastern edge of the B181 Epping Road south of the junction with Upland Road (see Image 1 below). Based on the location of the proposed give-way markings on plan provided to the Audit Team, this vegetation is likely to obstruct visibility to northbound vehicles therefore it may increase the risk of a failure to give-way type collision. If there are issues with vehicles clearing the junction it may also result in an increase in the risk of a secondary collision whereby the visibility of eastbound motorists travelling on Epping Road is likely to be obstructed by the brick wall on the offside and potentially by any vehicles travelling in the opposing direction.



Image 1 – Give-way line proposed left of this position where vegetation will obstruct visibility to northbound vehicles.

4. Initial Site Assessment

4.1. *Five year Collision History*

A study of the five-year collision history ending 30th June 2014 at the junction showed there have been three Personal Injury Collisions (PICs). A brief summary of the collisions is given below:

- 04/10 - Vehicle 1 turns right into B181 Epping Road across the path of a westbound vehicle.
- 03/11 - Vehicle loses control on icy patch of carriageway turning into the junction. Following vehicles also lose control on icy patch and collide into the first vehicle.
- 09/13 - Vehicle 2 waiting at the give-way line on B181 Epping Road to turn right when the front of motorcycle is clipped by vehicle 1 turning right into the junction.

4.2. *Other Site Assessments*

To carry out the following surveys and advise client prior to informal scheme consultation if any.

1. Current speed and volume survey – **To be requested (TBR) - subject to the scheme validation and approval of B181 junction Design.**
2. Pre-construction information survey - **TBR - subject to the scheme validation approval of B181 junction Design.**
3. Stats request - **TBR - subject to the scheme validation and approval of B181 junction Design.**
4. Residents survey - **TBR - subject to the scheme validation and approval of B181 junction Design.**

5. Recommendations

5.1. It is recommended after carrying out Auto Track check, and taking in to account the Network Management and Road Safety comments, if the scheme is to progress it will be necessary to widen the B181 junction and the road section turning left from the existing give-way by approximately 110m westwards.

If the scheme is to progress it will be necessary for Essex to purchase part of the land to be able to carry out the work as The land on both sides of the B181 at Epping Upland belongs to private owners.

Vehicles which are turning left from the existing give-way are over-running the opposing (eastbound) lane. It will be necessary to slow vehicles down in both directions before negotiating the bend as it is not possible to improve visibility across the bend given the brick boundary wall which runs adjacent to the carriageway.

5.2. The request to change the priority of the B181 junction with Upland Road, would not be appropriate.

As the proposed give-way line at Upland Road, will necessitate cutting back vegetation and bringing the give-way line forward as part of the junction realignment, this would result in narrowing the existing carriageway.

5.3. It is recommended that this scheme is not implemented as the existing layout is more suitable than the proposed layout, unless significant improvements to the junction are carried out.

Road users travelling northbound on B181 Epping Road must give-way due to poor visibility to the left before emerging on to B181 Epping Road/Upland Road and this manoeuvre was assessed to be generally reducing the risk of a collision at the junction. The scenario only appears to work because road users are forced to come to a stop before undertaking the left turn manoeuvre which reduces over-run of vehicles turning left and increases intervisibility between vehicles turning left on to B181 Epping Road and eastbound vehicles.

6. Addendum to Original Study

- 6.1. Further to issuing the original study the HLO for Epping Forest, the SMO3 Design Team Leader and Cllr Jackson met to discuss the original study and additional brief requirements on Wednesday 17th August 2016. The Design Team were asked to review the original study; investigate the feasibility of improving and/or increasing signage at the B181 junction with Upland Road; and investigate the feasibility of widening the existing carriageway on the bend adjacent to All Saints Church as it is considered that large vehicles encroach onto the existing grass verge at this location if two large vehicles pass each other at this point.
- 6.2. Having reviewed the original study the Design Team have no further comments to add to the original study. Taking the comments raised from the Stage 1 Road Safety Audit and Network Assurance it is still the view that reassigning the priority of the junction would result in safety concerns. Therefore it is still the recommendation of the Design Team that the priority continues to operate under the existing scenario than amend the priority to make Upland Road the minor road.
- 6.3. In order to investigate the feasibility of widening the carriageway around the bend adjacent to the Church a number of investigations have been undertaken. Firstly a site survey was undertaken to determine the accuracy of the existing Ordnance Survey (OS) base mapping in relation to the actual carriageway width. The site survey showed that the existing OS base map is not accurate. Subsequently the OS base map has been altered by the Design Team to represent a more accurate site scenario and existing carriageway widths.
- 6.4. Further to this swept path vehicle tracks have been carried out using a large rigid goods vehicle to accurately represent how large vehicles would likely manoeuvre around the bend. Further to these two assessments it was possible to determine the extent of verge that would be required to convert into carriageway in order for larger goods vehicles to manoeuvre around the bend. The extent of required verge is shown on drawing DC1785/000/001 and equates to a strip of around 55m in length by around 1m in width. This area would broadly coincide with the area of verge that is rutted on site.
- 6.5. In order to determine the further feasibility of widening the carriageway two further assessments have been made. Firstly the highway boundary has been attained and overlaid onto drawing DC1785/000/001. This shows that the proposed widening area would fit within the existing highway boundary. Secondly plans of utility services have been ascertained to determine if there would be any conflict with underground services. Analysis of the utility service plans shows that a BT cable does run within close proximity of the western kerb line along the B181 adjacent to the Church. It is unclear whether this service is within the carriageway or verge but plans do show that the plant crosses the carriageway at a number of locations. Therefore it is assumed that there would already be sufficient cover for this apparatus and that it would not need to be lowered further. Further investigations will be carried out into the Detailed Design process if the scheme progresses however.
- 6.6. The existing signage at the B181 junction with Upland Road has been reviewed with a view to making it more conspicuous and ensuring that vehicles do not incorrectly travel along Upland Road instead of continuing along the B181. It is considered that signage is perfectly adequate for vehicles travelling from the south towards Roydon and Epping Green. As Image 2 shows signage is sufficiently conspicuous highlighting both the B181 and the weight limit. The signage of the weight limit itself is also sufficiently conspicuous when travelling from the east.

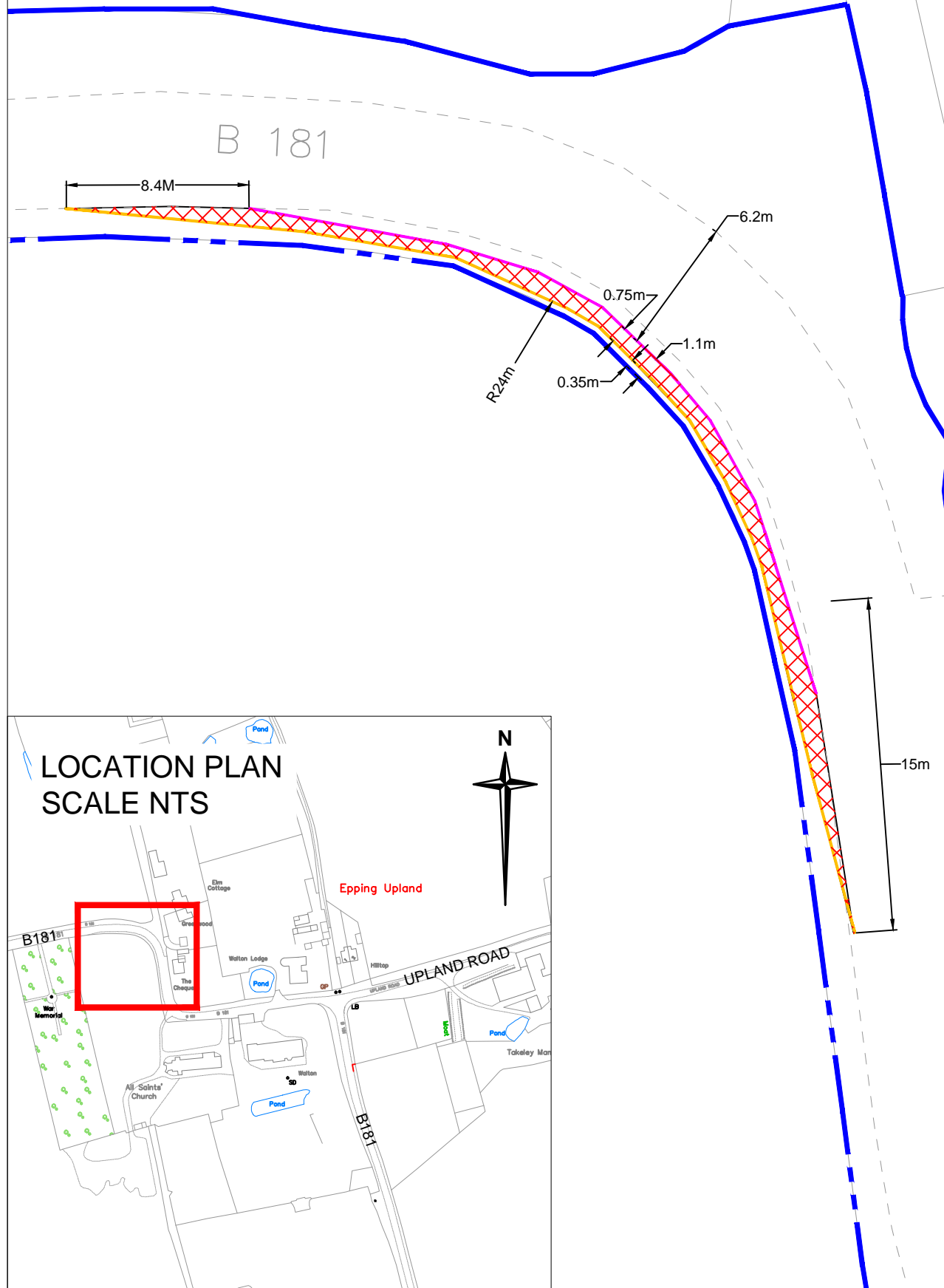


Image 2

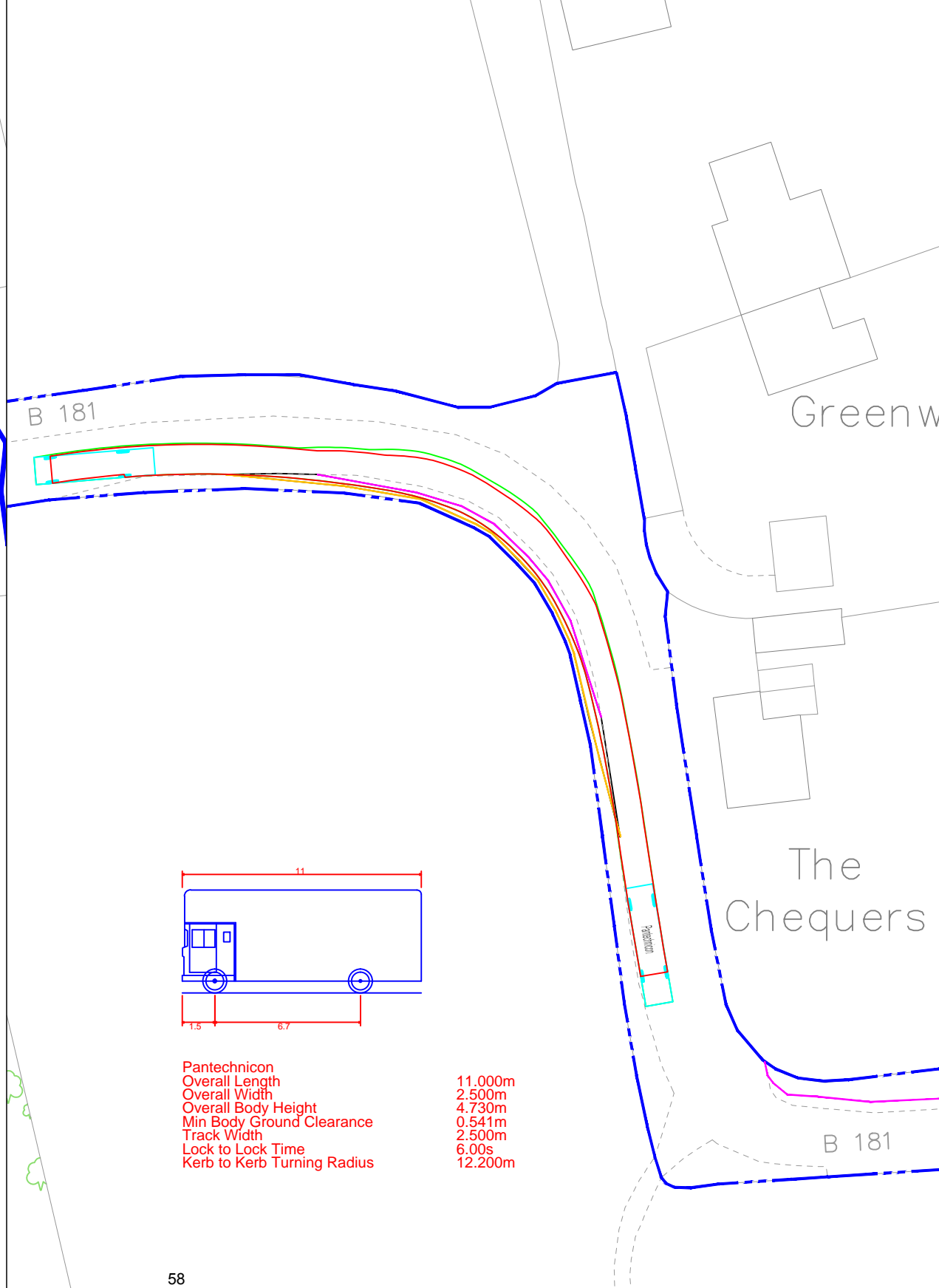
- 6.7 It is considered that there is sufficient advanced directional signage for southbound traffic on the approach to the junction as an advanced arrow sign exists directly outside of the Church entrance. It is the understanding of the Design Team that signage for southbound traffic did exist on the eastern verge at the junction but is not present anymore. The Design Team have investigated whether signage could be located elsewhere at the junction in order to make it more conspicuous but due to the lack of available land and the required lateral clearance for signage it is still considered that the eastern verge is the most suitable point to locate signage. Appropriate signage has been designed taking the available highway space and required visibility into account and is shown on drawing DC1785/000/002.
- 6.8 In conclusion both the widening works and signage replacement are feasible. The signage element of the scheme can be progressed without further investigations. It is recommended that further design work be carried out for the widening element of the scheme. In order to accurately carry out horizontal & vertical alignments, pavement and drainage design for the widening element it is recommended to carry out a topographical survey and engage with Asset Management. A Road Safety Audit will also be required as well as ensuring there will be no impact on Utility Services. For these reasons it is recommended to carry out Detailed Design in 2017/18 before implementation in 2018/19. Estimated costs are as follows:
- Estimated costs for widening element of scheme:
 - Topographical Survey - £1,000
 - Further Design Costs - £3,000
 - Road Safety Audit Costs - £500
 - Total Costs - £4,500
 - Estimated costs for signage replacement:
 - Design and RSA costs - £1,000
 - Works costs - £1,500
 - Total Costs - £2,500



EXISTING & PROPOSED KERB LINES
SCALE 1:250



SWEPT PATH MOVEMENTS ALONG PROPOSED KERB LINE
SCALE 1:500



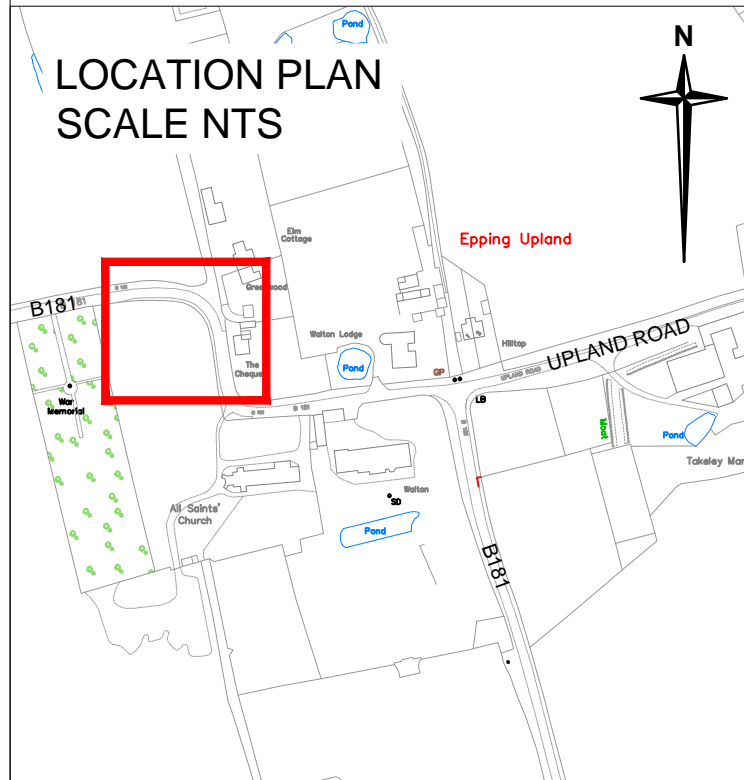
Notes

1. Do not scale.
2. This drawing is for information only.
3. The OS Plan is slightly inaccurate. Actual carriageway widths and kerb lines have been measured on site and adapted in the drawing. This is shown on the drawing and in the key.
4. Approximate area of carriageway new construction is 40m².
5. Approximate length of new kerbs is 60m.

Key

- Actual Kerbline
- Proposed New Kerbline
- OS Base Kerb Line
- Proposed Carriageway Construction
- Highway Boundary

LOCATION PLAN
SCALE NTS



Rev	Date	Description of revision	Drawn	Checked	Reviewed	Approved

DRAWING STATUS
FEASIBILITY



Mark Rowe, Service Director, Highways
Seax House, Victoria Road South, Chelmsford, CM1 1QH.
Tel: 0345 6037631 © Essex County Council

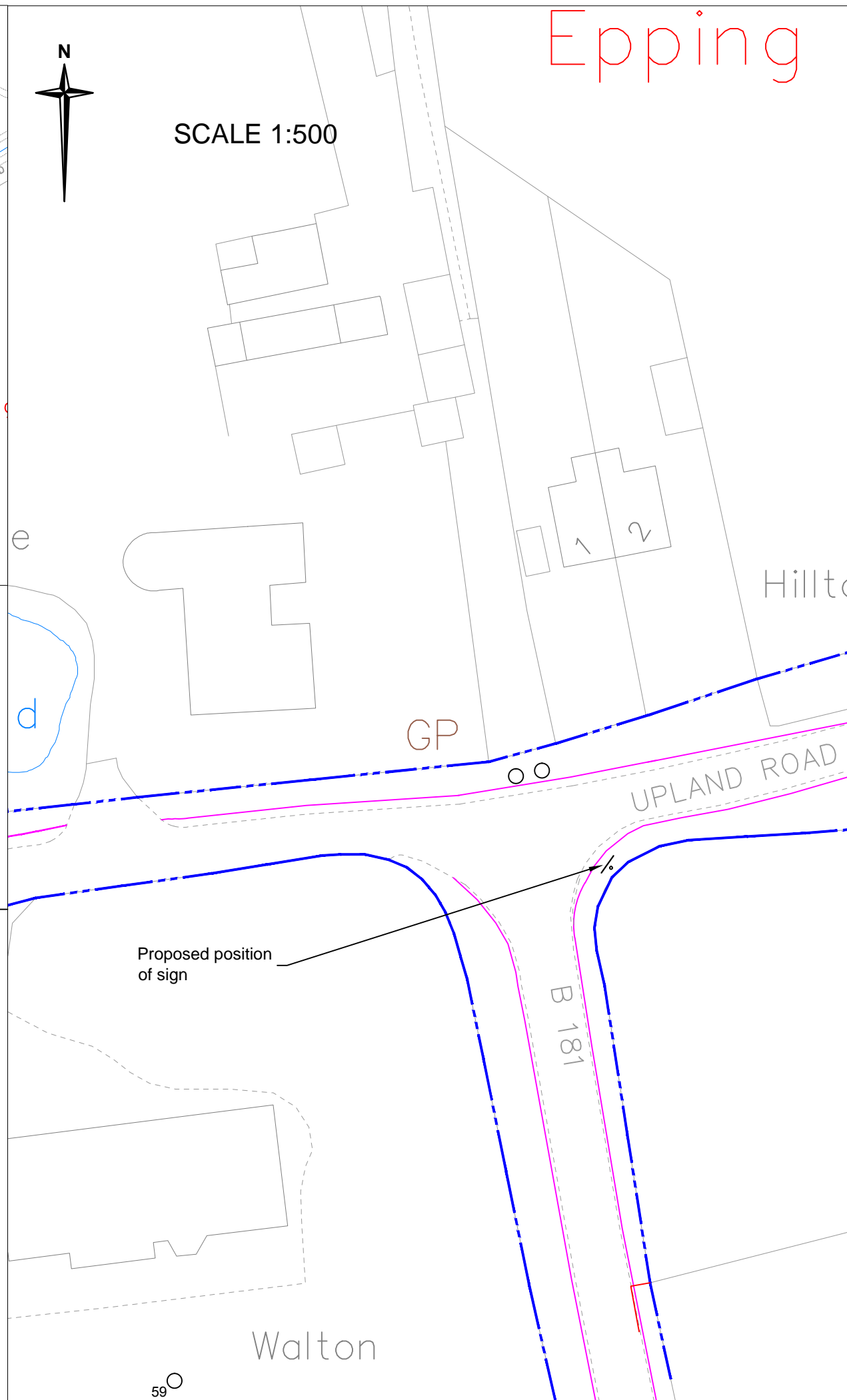
SCHEME TITLE
LOCAL HIGHWAYS PANEL
B181 EPPING UPLAND, EPPING

DRAWING TITLE
SWEPT PATH MOVEMENTS
FOR PROPOSED NEW KERB LINE

DESIGNED	DRAWN	CHECKED	REVIEWED	APPROVED
SM	SM	JOR	SM	JOR
DATE	DATE	DATE	DATE	DATE
FEB 17	FEB 17	MAR 17	MAR 17	MAR 17

DRAWING LIMITS U.N.O.
DIMENSIONS IN METRES
LEVELS IN METRES
SCALE AT A3 (420x297mm)
AS SHOWN

DRAWING No. **DC1785/000/001** REV. **-**



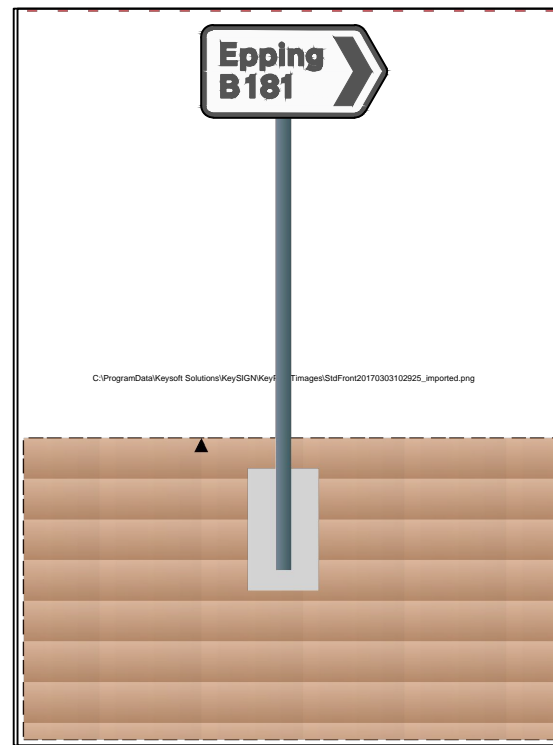
- Notes**
1. Do not scale.
 2. This drawing is for information only.

- Key**
- Actual Kerbline
 - OS Base Kerb Line
 - Highway Boundary
 - Proposed New Post & Sign



Sign to Dia 2125v (Sch.12.Pt.2.Item.4) to be mounted on new post.

Scheme Ref.	HI4264 B181/Upland Road, Epping		
Sign Ref.	Dia 2125v (Sch.12.Pt.2.Item.4)	x-height	75.0
Letter colour	BLACK	SIGN FACE	
Background	WHITE	Width	875mm
Border	BLACK	Height	430mm
Material	Class RA2 (12899-1:2007)	Area	0.38sq.m



POST(S) & FOUNDATIONS	
Assembly Name	DC1785 B181 - Upland Road
Author	AD
Reference	
Scheme Reference	DC1785
Description	
Illumination	No
Mounting Height	1600
Post Manufacturer	ASD Metal Services
Post Model	Steel Square post/Square Post
Post Type	Steel/S235;S235J2H;A283D
further post info	
Post Shape	Square
Post Size	76.0 x 76.0 x 3.0 thickness.
Number	1
Centres	-
Length	Total = 2631.3, above ground = 1981
No. of Foundations	1
Foundation Depth	600.0
Foundation Width	350.0
Foundation Length	350.0
Foundation Volume	0.074
Earth Cover	150.0

Rev	Date	Description of revision	Drawn	Checked	Review'd	Approv'd

DRAWING STATUS

FEASIBILITY

Mark Rowe, Service Director, Highways
Seax House, Victoria Road South, Chelmsford, CM1 1QH.
Tel: 0345 6037631 © Essex County Council

SCHEME TITLE

**LOCAL HIGHWAYS PANEL
B181 EPPING UPLAND, EPPING**

DRAWING TITLE

REPLACEMENT FLAG SIGN

DESIGNED	DRAWN	CHECKED	REVIEWED	APPROVED
AD	AD	SM	JOR	JOR

DATE	DATE	DATE	DATE	DATE
MAR 17	MAR 17	MAR 17	MAR 17	MAR 17

DRAWING UNITS IN METRES
LEVELS IN METRES

SCALE AT A3 (420x297mm)
AS SHOWN

DRAWING No. **DC1785/1200/002** REV. -